

#### NOTES:

1. ROADWAY STRUCTURAL SECTIONS SHOWN ARE MINIMUMS. ACTUAL STRUCTURAL COMPONENT VALUES SHALL BE DETERMINED BY SOILS/TRAFFIC ANALYSES FOR "R" VALUE AND TRAFFIC INDEX (TI). IN NO CASE SHALL THE ACTUAL STRUCTURAL SECTION BE LESS THAN THE MINIMUMS SHOWN.
2. PAVEMENT CROSS-SLOPES ARE MINIMUM-MAXIMUM VALUES.
3. SIDEWALK PATTERNS AND R/W WIDTH WILL VARY WITH LOCATION. SEE STD. DRAWING ST-5 FOR "CONCRETE CURB & GUTTER AND SIDEWALK."
4. FOG SEAL SHALL BE APPLIED WHEN SPECIFIED.
5. ALL WET UTILITY SPACING MUST COMPLY WITH THE STATE WATERWORKS STANDARDS (CALIFORNIA CODE OF REGULATIONS (CCR), TITLE 22, DIVISION 4, CHAPTER 16). SEE STANDARD SPECIFICATIONS AND STD DWG S-8, S-8A, AND S-9.
6. APPLY TACK COAT TO GUTTER PAN FACE PRIOR TO PAVING.



# CITY OF CLOVIS

## LOCAL STREET SECTIONS AND UTILITY LOCATIONS

DWG NO.  
**ST-1**

REF.: STD. SPECIFICATIONS  
SECTIONS 19, 26, 37,  
39, 73, APPENDIX

APPROVED BY:

CITY ENGINEER

DATE: 2020.12.10  
15:33:05-08'00"

NO.

REVISED

BY

APPROVALS

SCALE: NTS

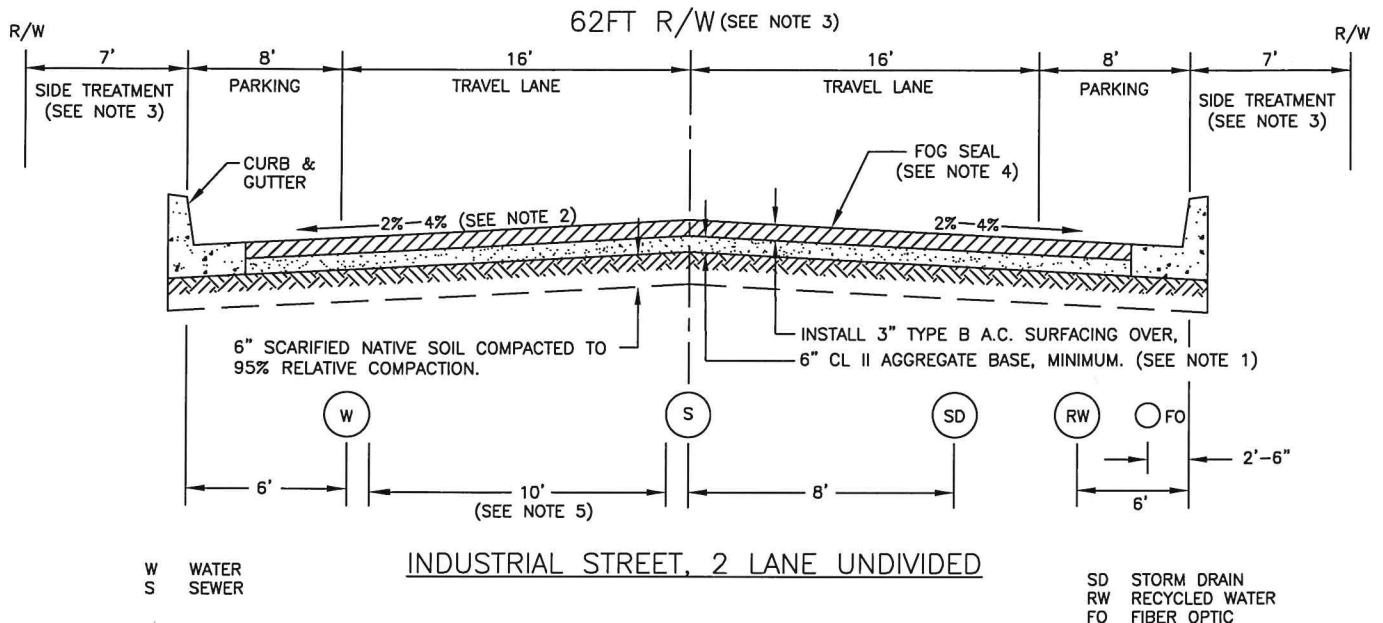
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11-22-11  
10-27-15  
02-11-20

PAA  
CGV  
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DRU  
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DRAWN BY: JA  
SHEET 1 OF 1



#### NOTES:

1. ROADWAY STRUCTURAL SECTIONS SHOWN ARE MINIMUMS. ACTUAL STRUCTURAL COMPONENT VALUES SHALL BE DETERMINED BY SOILS/TRAFFIC ANALYSES FOR "R" VALUE AND TRAFFIC INDEX (TI). IN NO CASE SHALL THE ACTUAL STRUCTURAL SECTION BE LESS THAN THE MINIMUMS SHOWN.
2. PAVEMENT CROSS-SLOPES ARE MINIMUM-MAXIMUM VALUES.
3. SIDEWALK PATTERNS AND R/W WIDTH WILL VARY WITH LOCATION. SEE STD. DRAWING ST-5 FOR "CONCRETE CURB & GUTTER AND SIDEWALK."
4. FOG SEAL SHALL BE APPLIED WHEN SPECIFIED.
5. ALL WET UTILITY SPACING MUST COMPLY WITH THE STATE WATERWORKS STANDARDS (CALIFORNIA CODE OF REGULATIONS (CCR), TITLE 22, DIVISION 4, CHAPTER 16). SEE STANDARD SPECIFICATIONS AND STD DWG S-8, S-8A, AND S-9.
6. ADDITIONAL R/W MAY BE REQUIRED FOR BICYCLE LANES AND BUS TURNOUTS.
7. APPLY TACK COAT TO GUTTER PAN FACE PRIOR TO PAVING.



# CITY OF CLOVIS

## INDUSTRIAL STREET SECTION AND UTILITY LOCATIONS

DWG NO.

**ST-2**

REF.: STD. SPECIFICATIONS  
SECTIONS 19, 26, 37,  
39, 73, APPENDIX

APPROVED BY:

CITY ENGINEER

DATE: 15:32:41-08'00'

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APPROVALS

SCALE: NTS

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02-11-20

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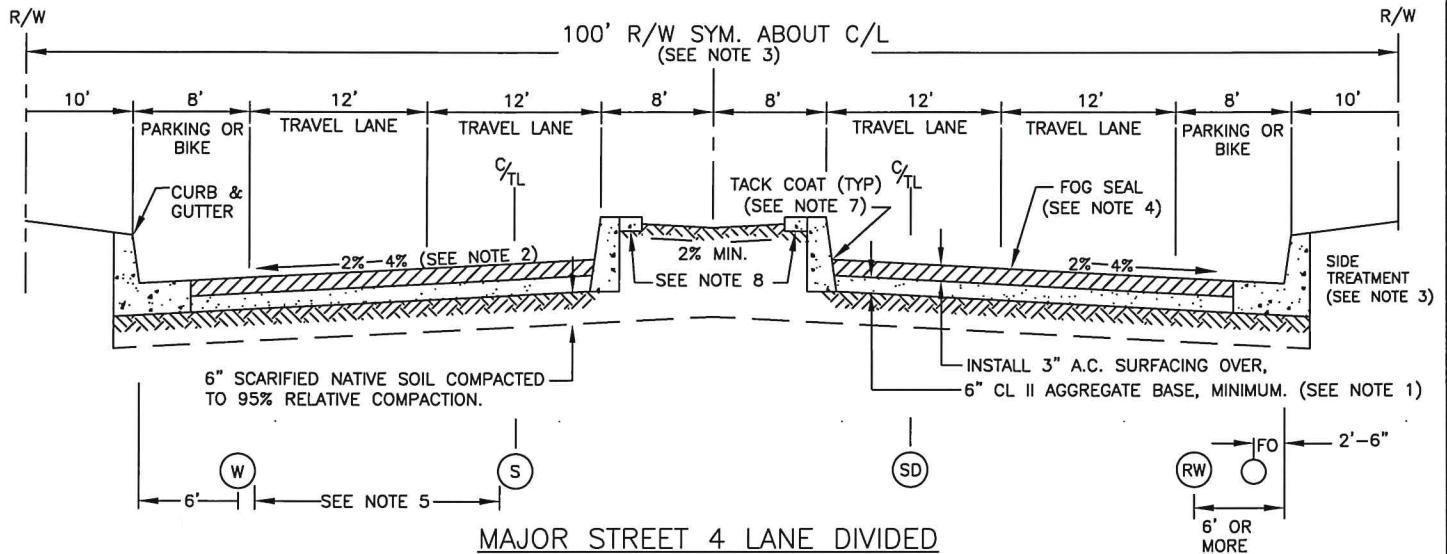
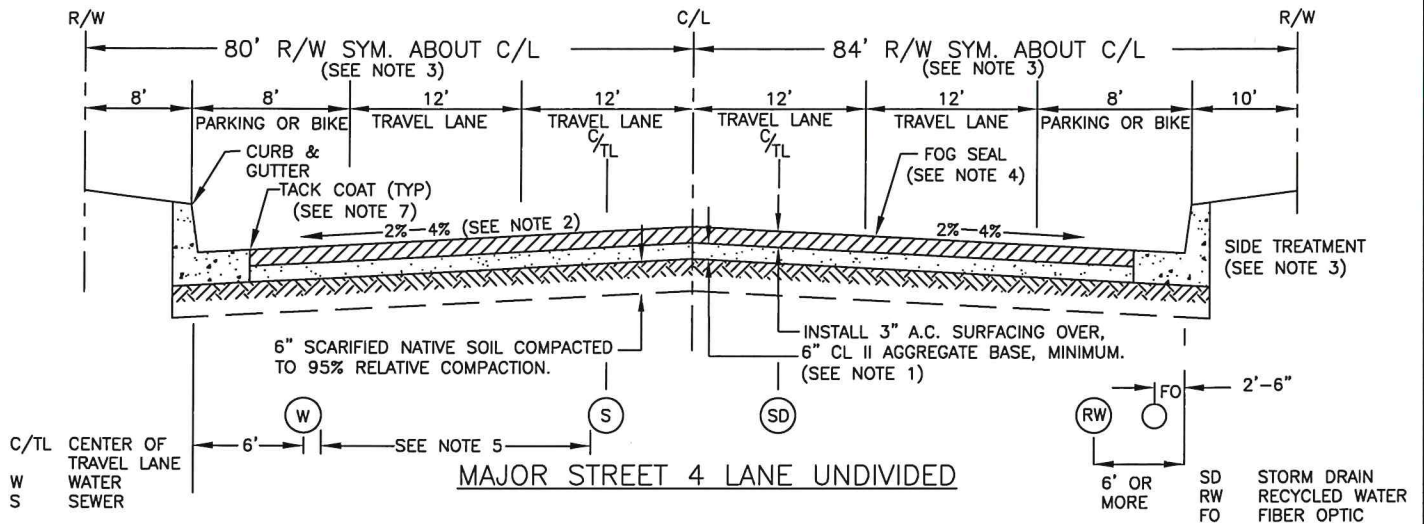
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DRAWN BY: JA

SHEET 1 OF 1



#### NOTES:

1. ROADWAY STRUCTURAL SECTIONS SHOWN ARE MINIMUMS. ACTUAL STRUCTURAL COMPONENT VALUES SHALL BE DETERMINED BY SOILS/TRAFFIC ANALYSES FOR "R" VALUE AND TRAFFIC INDEX (TI). IN NO CASE SHALL THE ACTUAL STRUCTURAL SECTION BE LESS THAN THE MINIMUMS SHOWN.
2. PAVEMENT CROSS-SLOPES ARE MINIMUM-MAXIMUM VALUES.
3. SIDEWALK PATTERNS AND R/W WIDTH WILL VARY WITH LOCATION. SEE STD. DRAWING ST-5 FOR "CONCRETE CURB & GUTTER AND SIDEWALK."
4. FOG SEAL SHALL BE APPLIED WHEN SPECIFIED.
5. ALL WET UTILITY SPACING MUST COMPLY WITH THE STATE WATERWORKS STANDARDS (CALIFORNIA CODE OF REGULATIONS (CCR), TITLE 22, DIVISION 4, CHAPTER 16). SEE STANDARD SPECIFICATIONS AND STD DWG S-8, S-8A, AND S-9.
6. ADDITIONAL R/W MAY BE REQUIRED FOR BICYCLE LANES AND BUS TURNOUTS.
7. APPLY TACK COAT TO GUTTER PAN/MEDIAN CURB FACE PRIOR TO PAVING.
8. THE MEDIAN SHALL INCLUDE AREA FOR A STAMPED CONCRETE MAINTENANCE STRIP BUT ONLY BE INSTALLED IF SO DIRECTED BY THE CITY ENGINEER.



# CITY OF CLOVIS

## FOUR LANE MAJOR STREET SECTIONS AND UTILITY LOCATIONS

DWG NO.

**ST-3**

REF.: STD. SPECIFICATIONS  
SECTIONS 19, 26, 37,  
39, 73, APPENDIX

APPROVED BY:

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BY

APPROVALS

SCALE: NTS

CITY ENGINEER

DATE: 15:33:40-08'00'

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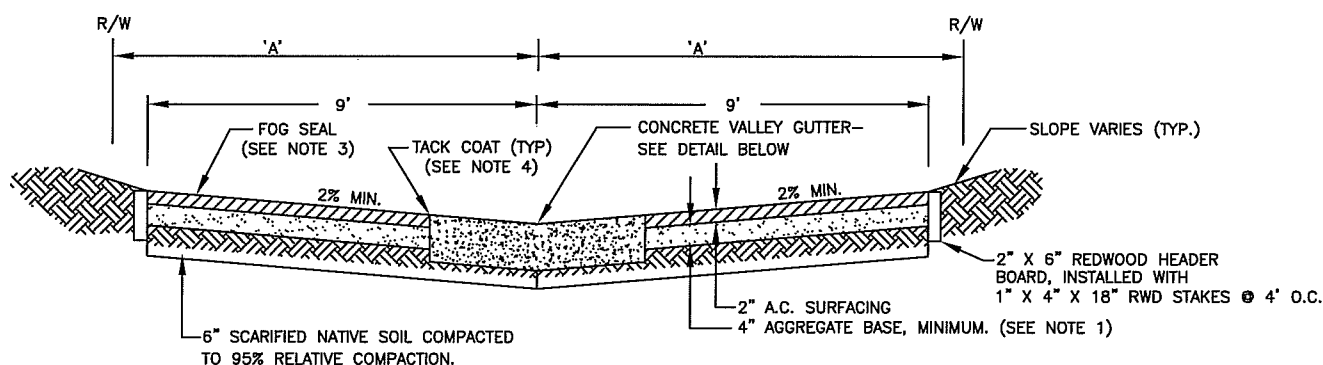
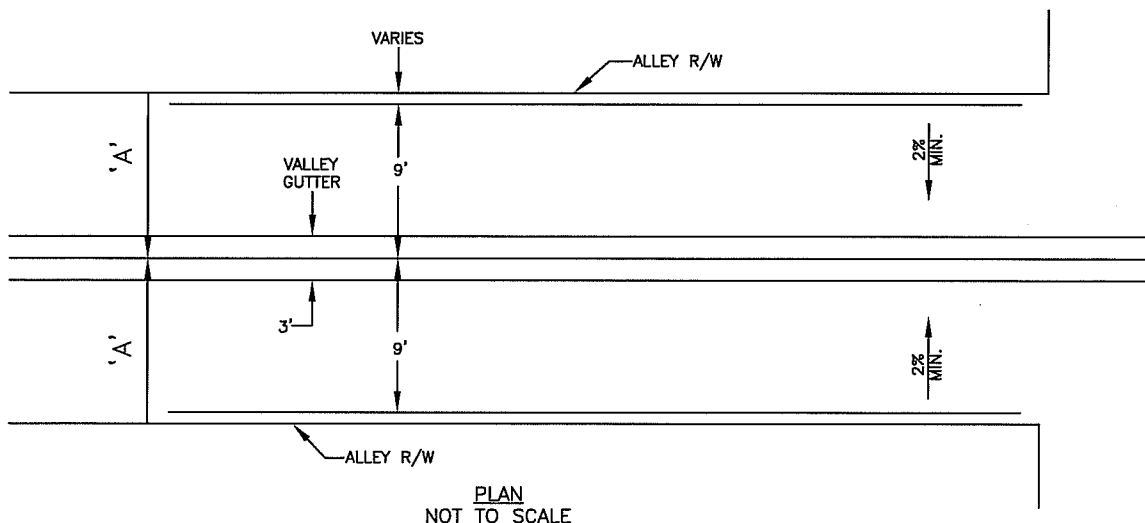
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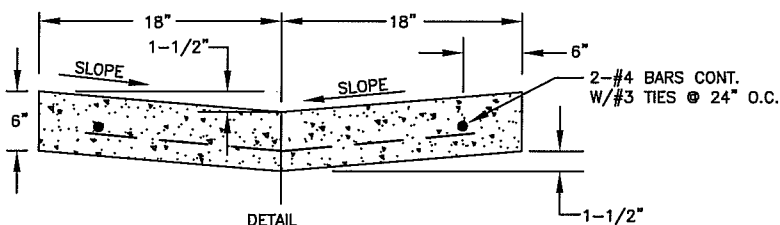
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SHEET 1 OF 1





'A'	LAND USE
10'	RESIDENTIAL
13'	MULTIPLE UNITS
15'	COMMERCIAL



PROVIDE WEAKENED PLANE JOINTS AT 15' O.C.  
AND EXPANSION JOINTS AT 45' O.C.  
CONCRETE SHALL BE CLASS 1 OR 2 CONCRETE

#### NOTES:

1. ALLEY STRUCTURAL SECTION SHOWN IS MINIMUM. ACTUAL STRUCTURAL COMPONENT VALUES SHALL BE DETERMINED BY SOILS/TRAFFIC ANALYSES FOR "R" VALUE AND TRAFFIC INDEX (TI), BUT IN NO CASE SHALL BE LESS THAN MINIMUMS SHOWN.
2. PRIOR TO ALLEY CONSTRUCTION, ALL WATER METER VALVE BOXES, SEWER MANHOLES & CLEANOUTS, OTHER UTILITIES TO BE NOTED AND PROTECTED, THEN RAISED/LOWERED TO FINISH GRADE FOLLOWING PAVING.
3. FOG SEAL SHALL BE APPLIED WHEN SPECIFIED.
4. APPLY TACK COAT TO GUTTER PAN FACE PRIOR TO PAVING.



# CITY OF CLOVIS

## ALLEY

DWG NO.

### ST-4

REF.: STD. SPECIFICATIONS

APPROVED BY:

NO.

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SCALE: NTS

CITY ENGINEER

DATE:

12-29-11

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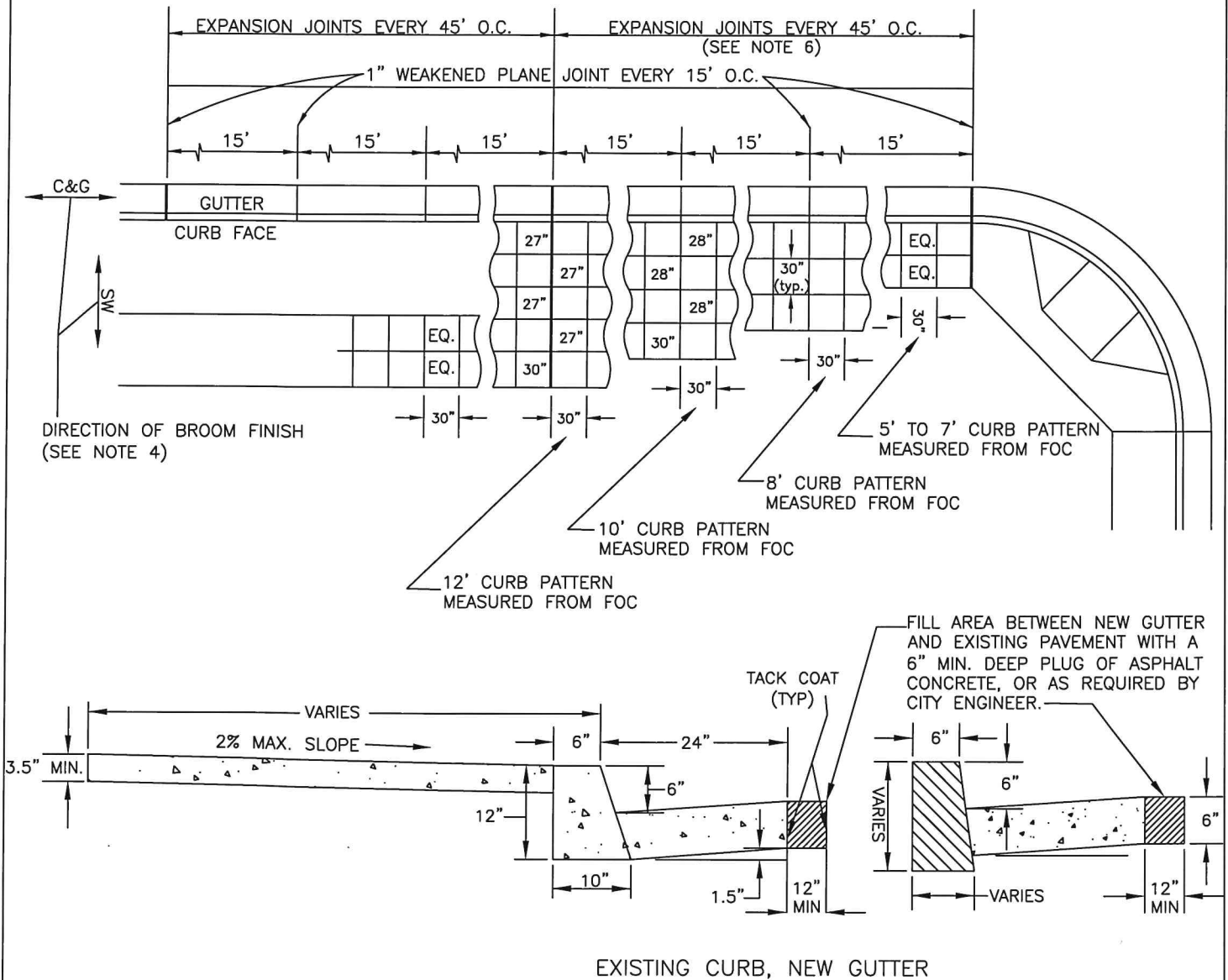
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SHEET 1 OF 1





#### NOTES:

1. SIDEWALKS, CURBS AND GUTTERS TO BE CLASS 3 CONCRETE; EXTRUDED CURB SHALL BE CLASS 2.
2. APPLY TACK COAT TO GUTTER PAN FACE AND EXISTING AC PRIOR TO PAVING.
3. BROOM FINISH ON C&G AND SIDEWALK. MED. SWEAT FINISH ON SIDEWALK IS OPTIONAL OR AS DIRECTED BY THE ENGINEER.
4. BROOM FINISH SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL ON SIDEWALKS, AND PARALLEL TO THE PATH OF TRAVEL ON CURB & GUTTER.
5. CURB & GUTTER SUBGRADE, COMPACT TO 95%. SIDEWALK SUBGRADE, COMPACT TO 90%; BEHIND DRIVE APPROACH, 95%.
6. EXPANSION JOINT MATERIAL SHALL CONSIST OF 1/4" THICK PREMOLDED JOINT MATERIAL MEETING ASTM DESIGNATION D-1751.
7. ALL WORK CONSTRUCTED BY THIS STANDARD SHALL BE COMPLETED IN COMPLIANCE WITH CURRENT ADA REGULATIONS.
8. THE SIDEWALK SHALL PROVIDE A CONTINUOUS AND UNOBSTRUCTED PATH-OF-TRAVEL FOR PEDESTRIANS WITH DISABILITIES IN ACCORDANCE WITH CURRENT ADA STANDARDS.
9. FINISH OF CONCRETE SHOULD MATCH ADJACENT PRE-EXISTING CONCRETE.
10. ANY UTILITY BOXES THAT ENCROACH THE SIDEWALK SHALL BE BORDERED BY A 12" CONCRETE COLLAR A MINIMUM OF 3 1/2" THICK.
11. ALL SIDEWALK REPLACEMENTS OR REPAIRS SHALL INSTALL #4 DOWEL BARS, 12" IN LENGTH AND TIE-IN TO EXISTING CONCRETE IMPROVEMENTS AT 18" O.C. OR AS DIRECTED BY THE CITY ENGINEER.



# CITY OF CLOVIS

## CONCRETE CURB & GUTTER AND SIDEWALKS

DWG NO.

# ST-5

REF.: STD. SPECIFICATIONS

APPROVED BY:

CITY ENGINEER

DATE: 4/5/21

NO.

REVISED

BY

APPROVALS

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04-13-17

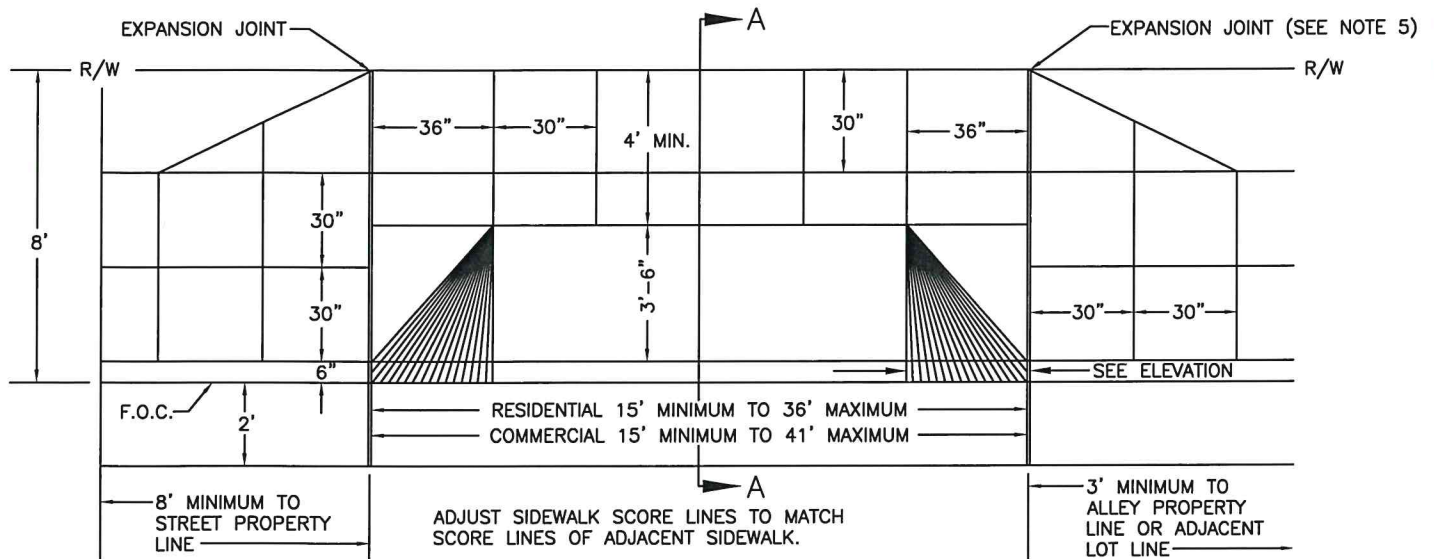
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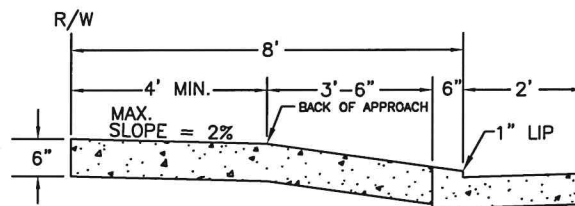
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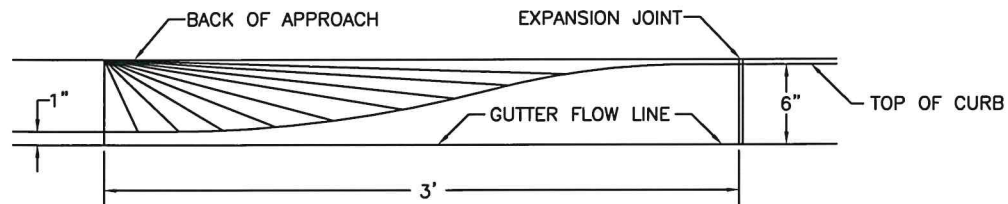




PLAN VIEW



SECTION A-A



ELEVATION

NOTES:

1. APPROACHES AND SIDEWALK SHALL BE CLASS 3 CONCRETE. THE APPROACH, SIDEWALK BEHIND APPROACH, AND CURB & GUTTER SUBGRADE SHALL BE COMPACTED TO 95%. OTHER SIDEWALK SUBGRADE SHALL BE COMPACTED TO 90%.
2. BROOM FINISH ON APPROACH AND ON SIDEWALK. MEDIUM SWEAT FINISH ON SIDEWALK IS OPTIONAL OR AS DIRECTED BY THE ENGINEER. DEEP SCORE MARK IN CENTER OF APPROACH WHEN THROAT IS WIDER THAN 20'.
3. BROOM FINISH SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL ON SIDEWALKS, AND PARALLEL TO THE PATH OF TRAVEL ON CURB & GUTTER.
4. NOT MORE THAN 60% OF CURB FACE MAY BE USED FOR DRIVEWAY APPROACH OPENING.
5. EXPANSION JOINT MATERIAL SHALL CONSIST OF 1/4" PREMOLDED JOINT FILLER MEETING ASTM DESIGNATION D 1751.
6. ALL WORK CONSTRUCTED BY THIS STANDARD SHALL BE COMPLETED IN COMPLIANCE WITH CURRENT ADA REGULATIONS.



# CITY OF CLOVIS

DWG NO.

ST-7

## DRIVEWAY APPROACH 8' CURB PATTERN

REF.: STD. SPECIFICATIONS

APPROVED BY:

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REVISED

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APPROVALS

SCALE: NTS

CITY ENGINEER

DATE: 15:28:52-08'00'

12-29-11

05-29-12

05-04-20

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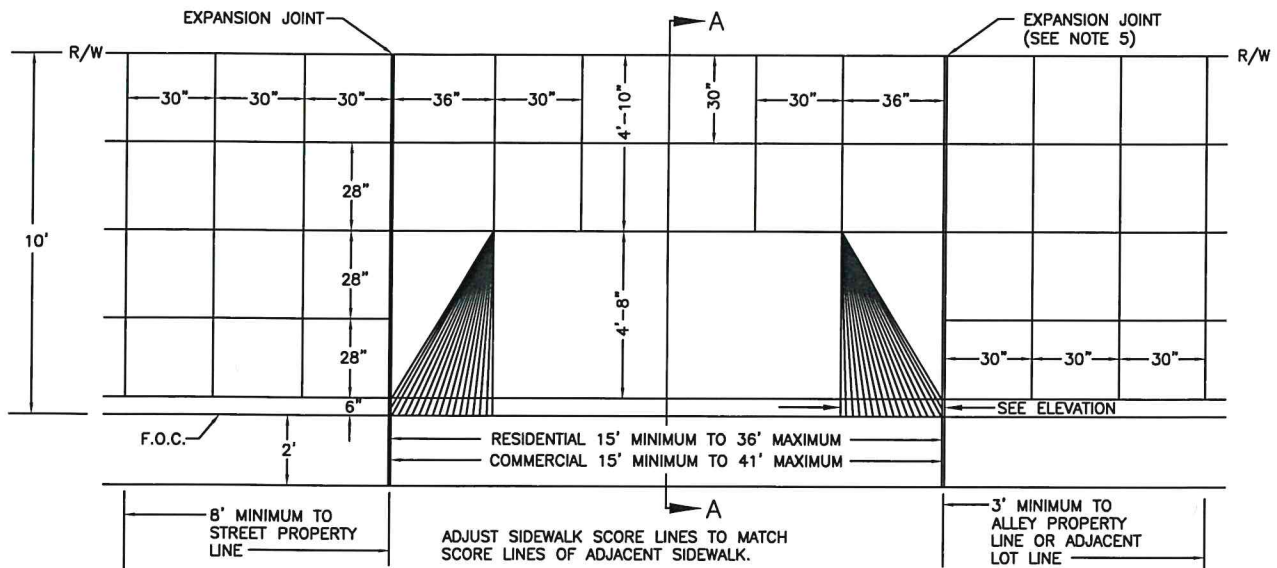
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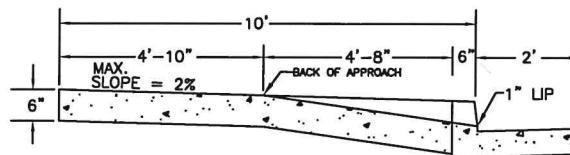
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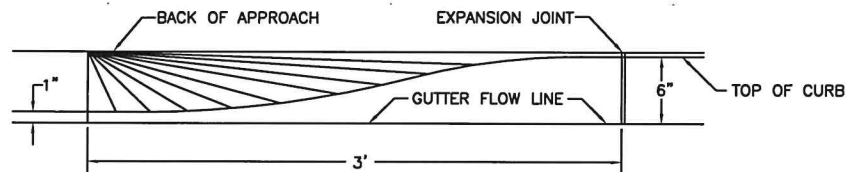




PLAN VIEW



SECTION A-A



ELEVATION

**NOTES:**

1. APPROACHES AND SIDEWALK SHALL BE CLASS 3 CONCRETE. THE APPROACH, SIDEWALK BEHIND APPROACH, AND CURB & GUTTER SUBGRADE SHALL BE COMPACTED TO 95%. OTHER SIDEWALK SUBGRADE SHALL BE COMPACTED TO 90%.
2. BROOM FINISH ON APPROACH AND ON SIDEWALK. MEDIUM SWEAT FINISH ON SIDEWALK IS OPTIONAL OR AS DIRECTED BY THE ENGINEER. DEEP SCORE MARK IN CENTER OF APPROACH WHEN THROAT IS WIDER THAN 20'.
3. BROOM FINISH SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL ON SIDEWALKS, AND PARALLEL TO THE PATH OF TRAVEL ON CURB & GUTTER.
4. NOT MORE THAN 60% OF CURB FACE MAY BE USED FOR DRIVEWAY APPROACH OPENING.
5. EXPANSION JOINT MATERIAL SHALL CONSIST OF 1/4" PREMOLDED JOINT FILLER MEETING ASTM DESIGNATION D 1751.
6. ALL WORK CONSTRUCTED BY THIS STANDARD SHALL BE COMPLETED IN COMPLIANCE WITH CURRENT ADA REGULATIONS.



# CITY OF CLOVIS

DWG NO.

**ST-8**

## DRIVEWAY APPROACH 10' CURB PATTERN

REF.: STD. SPECIFICATIONS

APPROVED BY:

CITY ENGINEER

DATE: 2020.12.10  
15:26:00-08'00'

NO.

REVISED

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APPROVALS

SCALE: NTS

12-29-11

05-04-20

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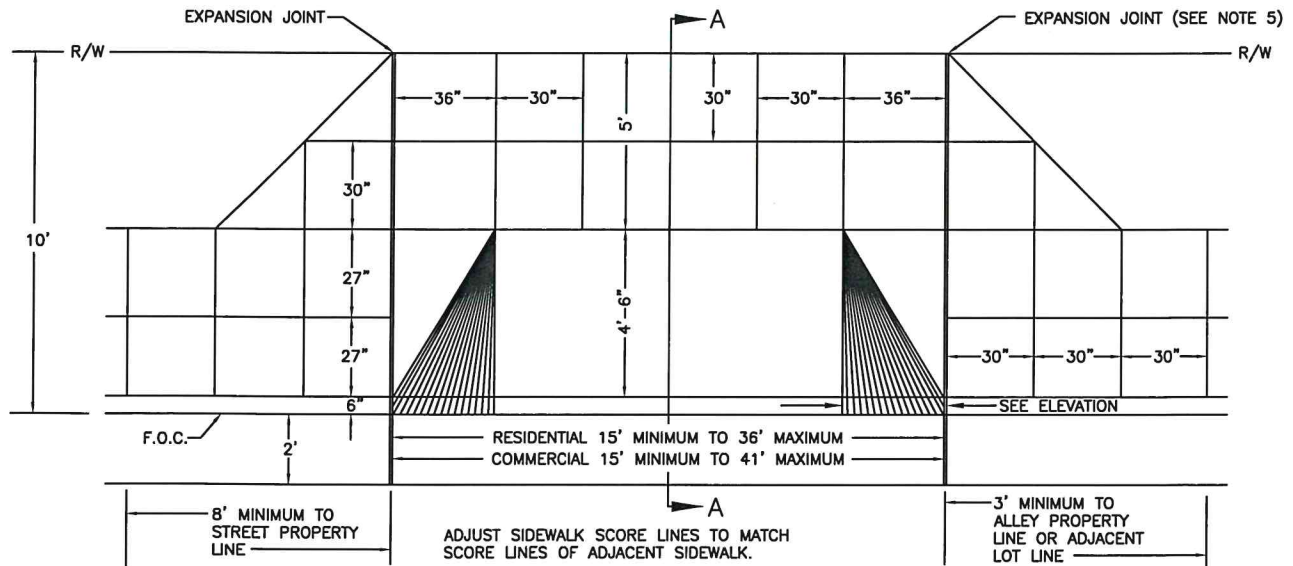
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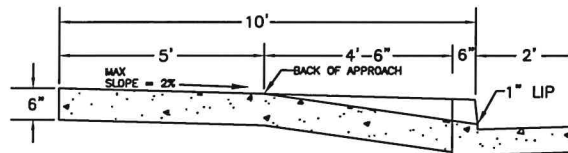
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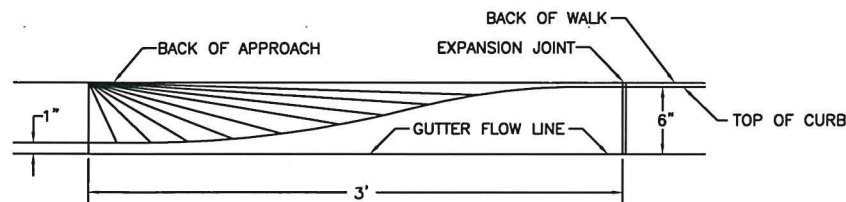
SHEET 1 OF 1



PLAN VIEW



SECTION A-A



ELEVATION

NOTES:

1. APPROACHES AND SIDEWALK SHALL BE CLASS 3 CONCRETE. THE APPROACH, SIDEWALK BEHIND APPROACH, AND CURB & GUTTER SUBGRADE SHALL BE COMPACTED TO 95%. OTHER SIDEWALK SUBGRADE SHALL BE COMPACTED TO 90%.
2. BROOM FINISH ON APPROACH AND ON SIDEWALK. MEDIUM SWEAT FINISH ON SIDEWALK IS OPTIONAL OR AS DIRECTED BY THE ENGINEER. DEEP SCORE MARK IN CENTER OF APPROACH WHEN THROAT IS WIDER THAN 20'.
3. BROOM FINISH SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL ON SIDEWALKS, AND PARALLEL TO THE PATH OF TRAVEL ON CURB & GUTTER.
4. NOT MORE THAN 60% OF CURB FACE MAY BE USED FOR DRIVEWAY APPROACH OPENING.
5. EXPANSION JOINT MATERIAL SHALL CONSIST OF 1/4" PREMOLDED JOINT FILLER MEETING ASTM DESIGNATION D 1751.
6. ALL WORK CONSTRUCTED BY THIS STANDARD SHALL BE COMPLETED IN COMPLIANCE WITH CURRENT ADA REGULATIONS.



# CITY OF CLOVIS

DWG NO.

ST-8A

## DRIVEWAY APPROACH ALT. 10' CURB PATTERN

REF.: STD. SPECIFICATIONS

APPROVED BY:

NO.

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APPROVALS

SCALE: NTS

CITY ENGINEER

2020.12.10

DATE: 15:29:54-08'00'

12-29-11

11-20-12

05-04-20

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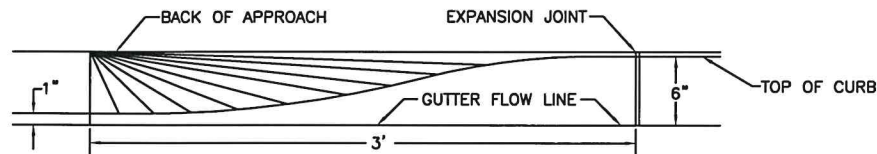
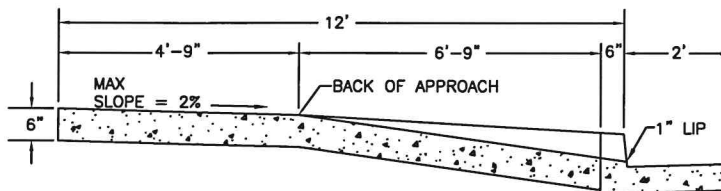
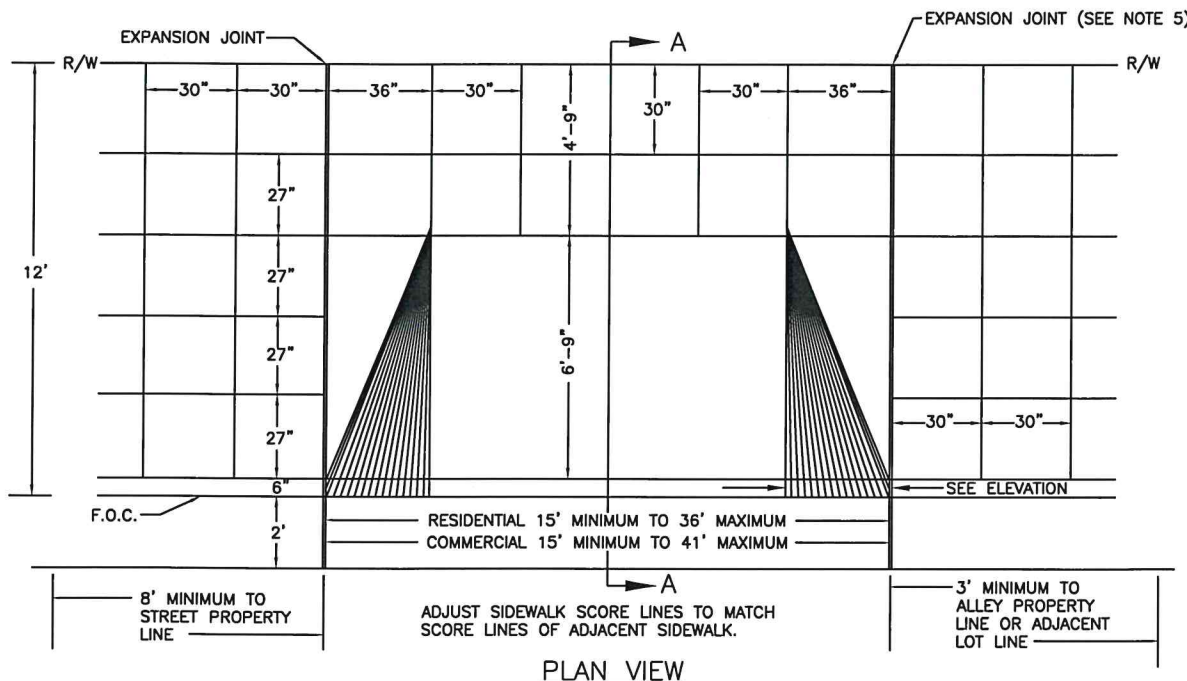
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SHEET 1 OF 1



#### NOTES:

1. APPROACHES AND SIDEWALK SHALL BE CLASS 3 CONCRETE. THE APPROACH, SIDEWALK BEHIND APPROACH, AND CURB & GUTTER SUBGRADE SHALL BE COMPACTED TO 95%. OTHER SIDEWALK SUBGRADE SHALL BE COMPACTED TO 90%.
2. BROOM FINISH ON APPROACH AND ON SIDEWALK. MEDIUM SWEAT FINISH ON SIDEWALK IS OPTIONAL OR AS DIRECTED BY THE ENGINEER. DEEP SCORE MARK IN CENTER OF APPROACH WHEN THROAT IS WIDER THAN 20'.
3. BROOM FINISH SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL ON SIDEWALKS, AND PARALLEL TO THE PATH OF TRAVEL ON CURB & GUTTER.
4. NOT MORE THAN 60% OF CURB FACE MAY BE USED FOR DRIVEWAY APPROACH OPENING.
5. EXPANSION JOINT MATERIAL SHALL CONSIST OF 1/4" PREMOLDED JOINT FILLER MEETING ASTM DESIGNATION D 1751.
6. ALL WORK CONSTRUCTED BY THIS STANDARD SHALL BE COMPLETED IN COMPLIANCE WITH CURRENT ADA REGULATIONS.



# CITY OF CLOVIS

## DRIVEWAY APPROACH 12' CURB PATTERN

DWG NO.

**ST-9**

REF.: STD. SPECIFICATIONS

APPROVED BY:

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SCALE: NTS

CITY ENGINEER

DATE: 15:31:23-08'00'

12-29-11

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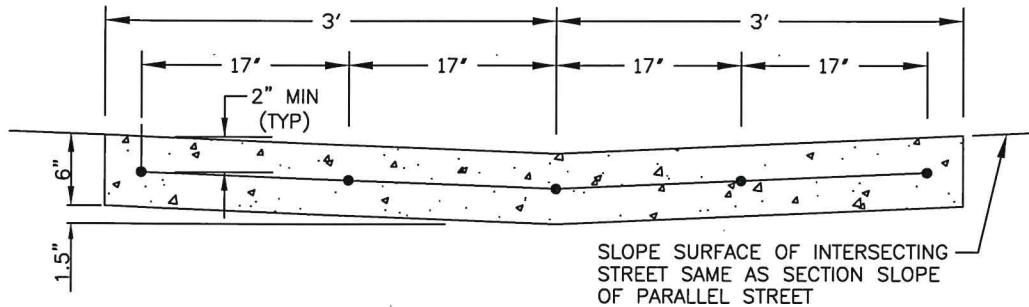
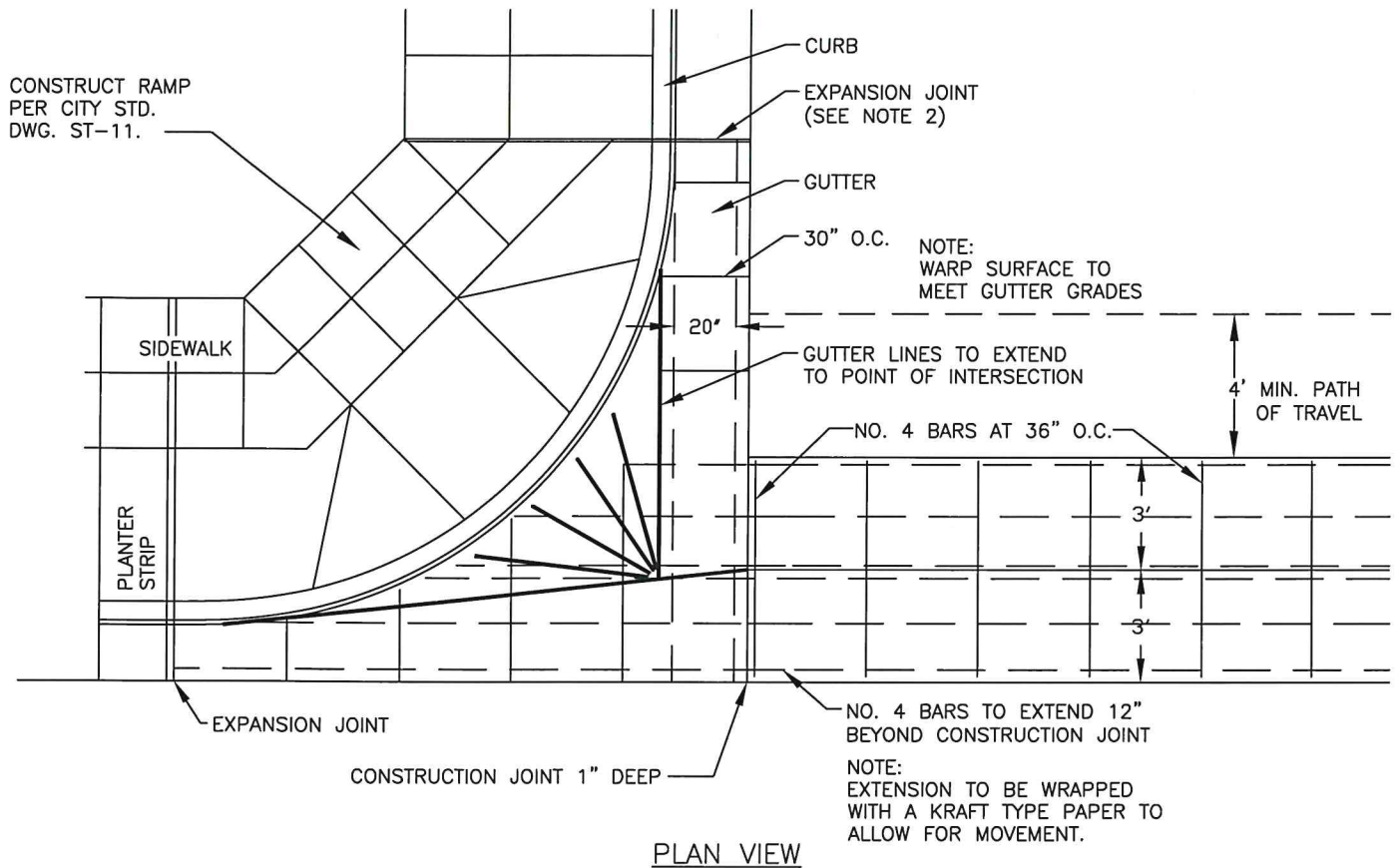
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SHEET 1 OF 1





SECTION

NOTES:

1. CURB RETURN, GUTTER AND VALLEY GUTTER TO BE POURED MONOLITHICALLY.
2. EXPANSION JOINT MATERIAL SHALL BE 1/4" THICK PREMOLDED JOINT FILLER CONFORMING TO REQUIREMENTS OF ASTM DESIGNATION D 1751.
3. SURFACE SHALL BE ROUGH BROOM FINISH.
4. CONCRETE SHALL BE CLASS I OR 2.
5. VALLEY GUTTER SUBGRADE SHALL BE COMPACTED TO 95%.



# CITY OF CLOVIS

## CONCRETE VALLEY GUTTER

DWG NO.  
**ST-10**

REF.:  
STD. SPECIFICATIONS

APPROVED BY:

CITY ENGINEER

DATE: 15:34:03-08'00'

NO.

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APPROVALS

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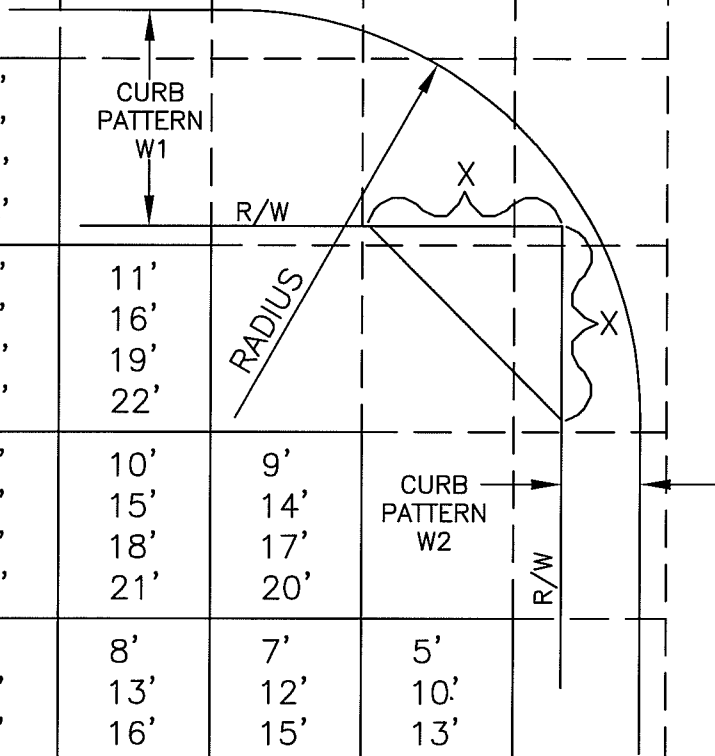
SHEET 1 OF 1



PROPERTY CORNER CUTOFFS REQUIRED TO ACCOMODATE RAMPS AT CURB RETURNS.

ALL RETURNS ASSUMED TO BE 90°. FORMULA:  $X = \frac{R(\sec 45 - 1) + 12.5}{\sin 45} - (W1 + W2)$

CURB PATTERN(W)	RADIUS	5'	6'	7'	8'	10'	12'
5'	12' 20' 25' 30'	15' 20' 23' 26'					
6'	12' 20' 25' 30'	14' 19' 22' 25'	13' 18' 21' 24'				
7'	12' 20' 25' 30'	13' 18' 21' 24'	12' 17' 20' 23'	11' 16' 19' 22'			
8'	12' 20' 25' 30'	12' 17' 20' 23'	11' 16' 19' 22'	10' 15' 18' 21'	9' 14' 17' 20'		
10'	12' 20' 25' 30'	10' 15' 18' 21'	9' 14' 17' 20'	8' 13' 16' 19'	7' 12' 15' 18'	5' 10' 13' 16'	
12'	12' 20' 25' 30'	8' 13' 16' 19'	7' 12' 15' 18'	6' 11' 14' 17'	5' 10' 13' 16'	3' 8' 11' 14'	— 6' 9' 12'
16'	12' 20' 25' 30'	4' 9' 12' 15'	3' 8' 11' 14'	2' 7' 10' 13'	1' 6' 9' 12'	— 4' 7' 10'	— 2' 5' 8'



ALL FIGURES ROUNDED UP TO THE NEAREST FOOT.



# CITY OF CLOVIS

## CORNER CUTOFF COMPUTATIONS

DWG NO.

ST-12

REF.:

APPROVED BY:

NO.

REVISED

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APPROVALS

SCALE: NTS

CITY ENGINEER

DATE:

01-13-11

02-16-11

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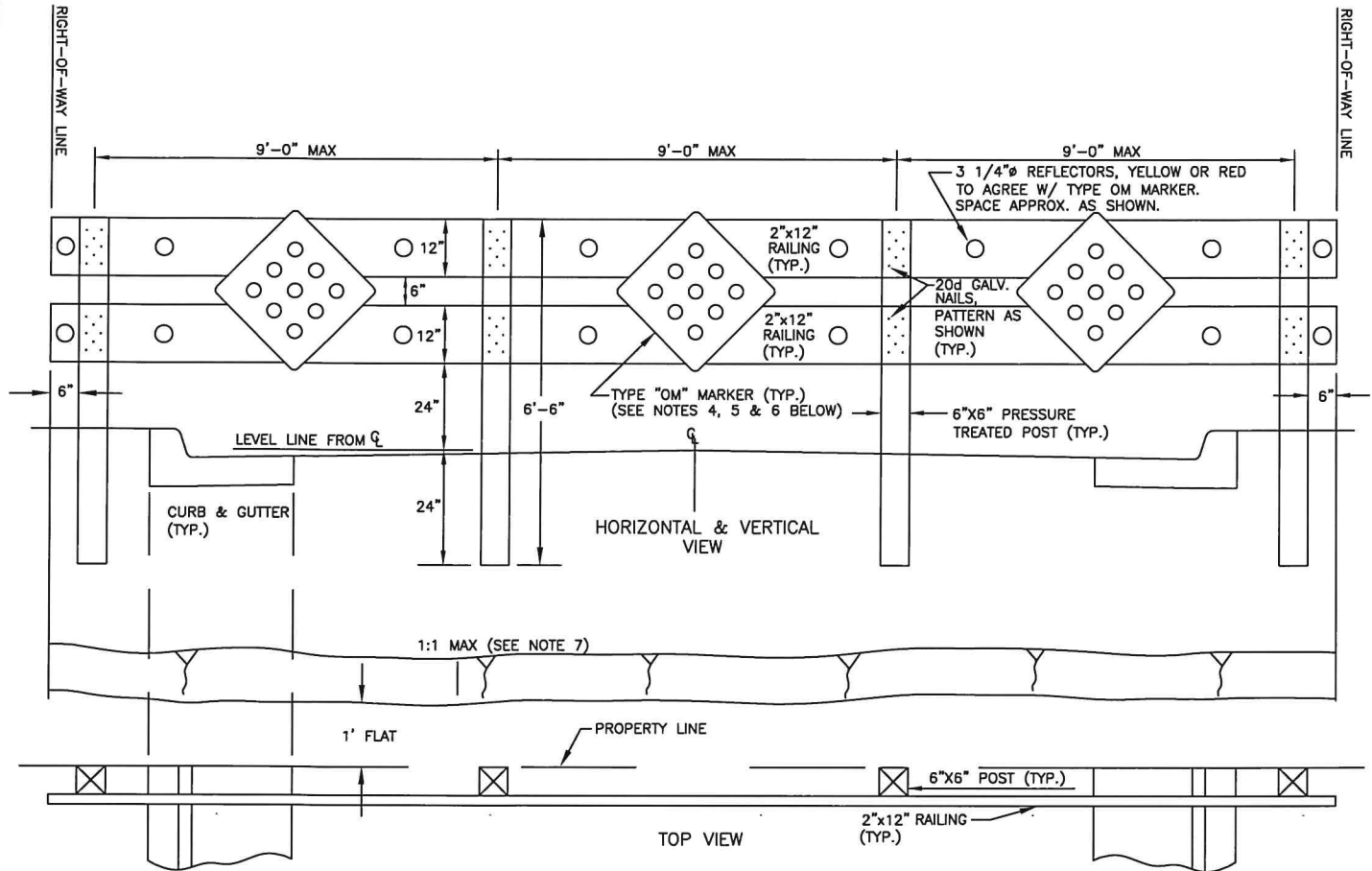
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SHEET 1 OF 1





#### NOTES:

1. TIMBER: POSTS — PRESSURE TREATED HEM FIR.  
RAILING — HEM FIR.
2. PAINT: TWO COATS OF 100% ACRYLIC "SCHOOL BUS" YELLOW PAINT SHALL BE APPLIED TO ALL WOOD SURFACES.
3. BARRICADE MUST BE INSTALLED FULL WIDTH OF RIGHT-OF-WAY. ADD SECTIONS AS NECESSARY.
4. TYPE OM1-1 YELLOW MARKER USED TO WARN OF UNIMPROVED CONDITIONS AHEAD.
5. TYPE OM4-1 RED MARKER USED TO MARK THE END OF A STREET.
6. TYPE OM MARKERS TO CONFORM CAL MUTCD SPECIFICATIONS.
7. FOR DIFFERENCE IN ELEVATION GREATER THAN 6" BETWEEN FRONT AND REAR OF BARRICADE, GRADE AS SHOWN.
8. WHERE RETAINING WALL IS LOCATED ADJACENT TO BARRICADE, BARRICADE SHALL BE SEPARATED FROM WALL AND NOT INSTALLED AS PART OF THE WALL.



# CITY OF CLOVIS

## TEMPORARY TIMBER BARRICADE

DWG NO.  
**ST-13**

REF.: STD. SPECIFICATIONS

APPROVED BY:

CITY ENGINEER

DATE: 4/5/21

NO.

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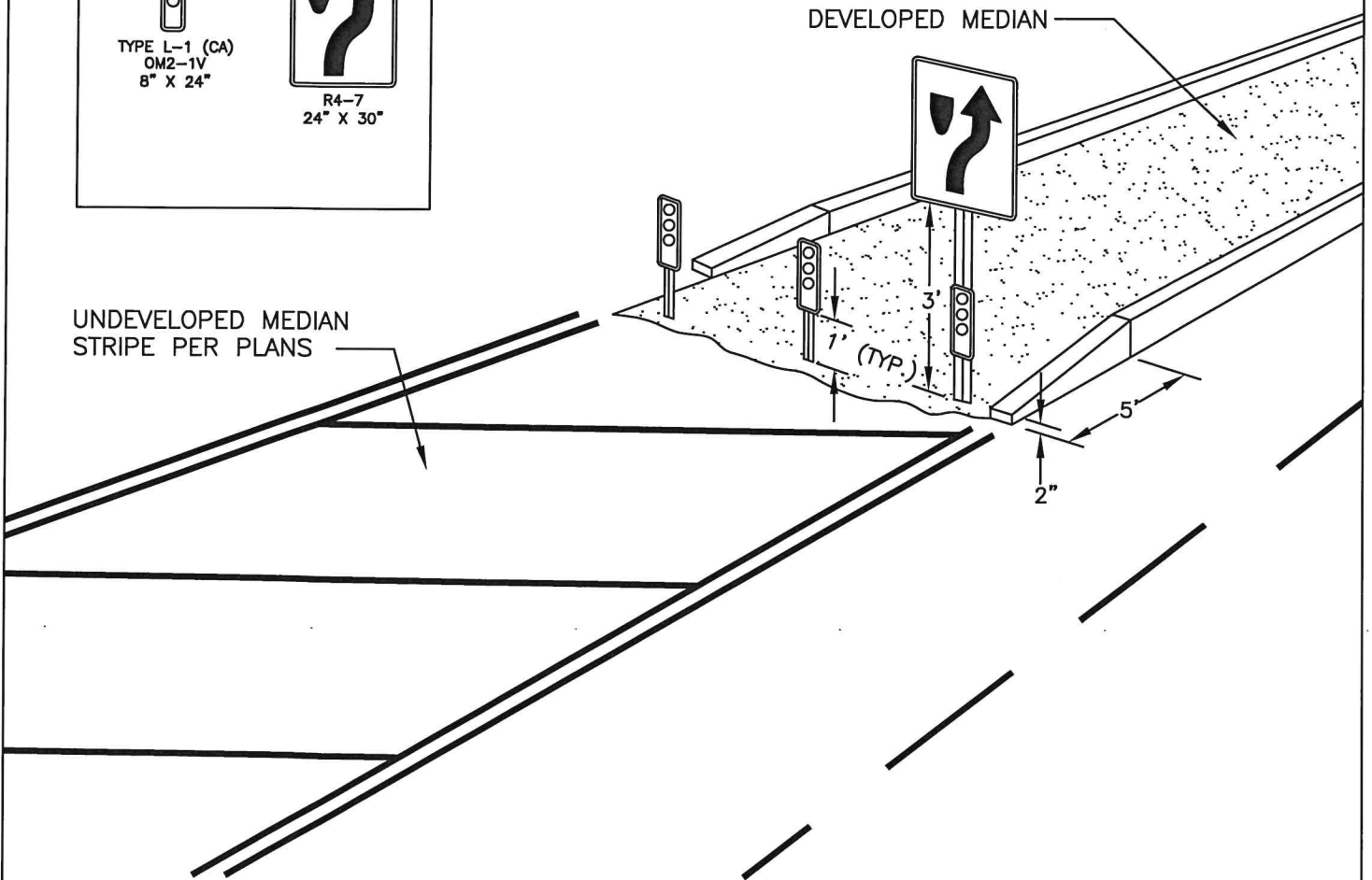
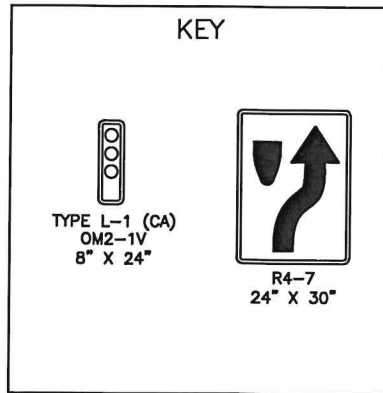
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SHEET 1 OF 1

**NOTE:**

SIGN POSTS SHALL BE 2" X 2" X 12 GAUGE  
SQUARE GALVANIZED STEEL, PERFORATED.



# CITY OF CLOVIS

## MEDIAN TRANSITION STANDARD

DWG NO.  
**ST-14**

REF.:  
STD. SPECIFICATIONS  
SECTION 84

APPROVED BY:

CITY ENGINEER

DATE: 4/5/21

NO.

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BY

APPROVALS

05-20-09

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01-14-11

BGJ

DRU

03-27-19

CGV

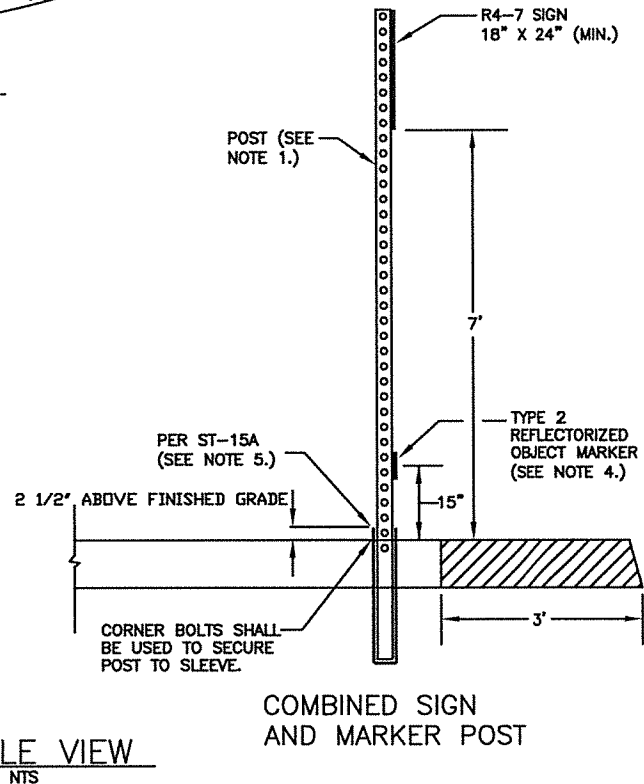
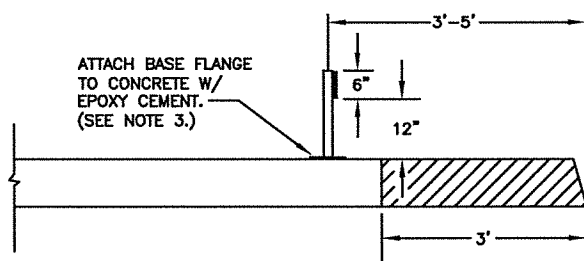
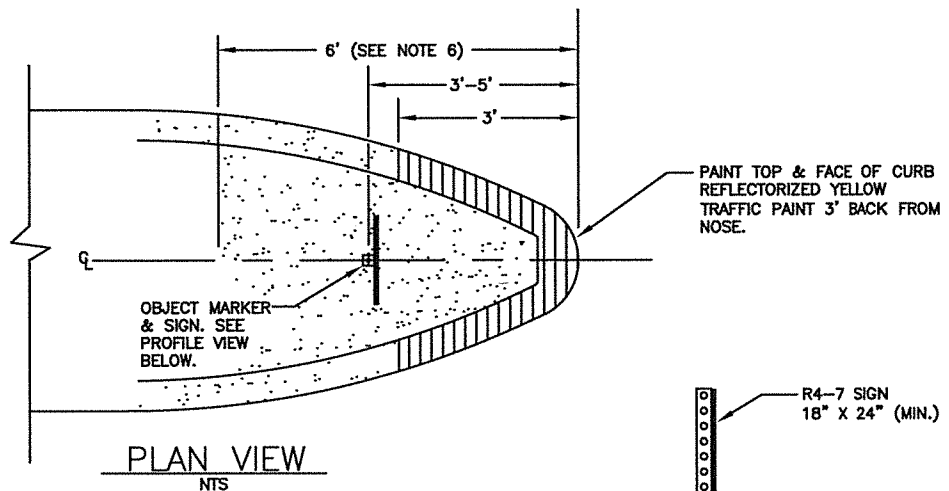
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SCALE: NTS

DRAWN BY: JA

SHEET 1 OF 1



### NOTES:

1. POSTS FOR COMBINED STREET SIGN AND OBJECT MARKER SHALL BE 12 GAUGE 2" X 2" SQUARE, GALVANIZED AND PERFORATED.
2. FOR POSTS TO BE USED IN COMBINATION WITH PEDESTRIAN PUSH BUTTON POLES, REFER TO TRAFFIC SIGNAL STD. DRAWINGS NO. TS-7 & TS-7A.
3. POST AND FLANGE FOR TYPE 2 (OM2-1H) OBJECT MARKER SHALL BE SCHED. 80 PVC, 2-1/4" O.D. MIN.
4. OBJECT MARKER SHALL BE TYPE 2, REFLECTORIZED (YELLOW REFLECTORS) CONFORMING TO CAL MUTCD OM2-1H.
5. ANCHOR SHALL BE 30" "ANCHOR-MATE W/ 12" SLEEVE", ULTIMATE HIGHWAY PRODUCTS, INC. OR APPROVED EQUAL. SEE STD. DRAWING NO. ST-15A. ANCHOR TO BE INSTALLED NO MORE THAN 2 1/2" ABOVE GRADE.
6. INSTALL 3" THICK CLASS 3 CONCRETE CAP MINIMUM 6' FROM NOSE. CERTAIN LOCATIONS MAY REQUIRE STAMPED CONCRETE.



# CITY OF CLOVIS

## MEDIAN ISLAND NOSE MARKER & SIGN

DWG NO.

ST-15

REF.:  
STD. SPECIFICATIONS

APPROVED BY:

NO.

REVISED

BY

APPROVALS

SCALE: NTS

CITY ENGINEER

DATE: 6/6/19

01-14-11

05-29-12

09-28-12

03-20-19

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DRAWN BY: JA

SHEET 1 OF 1



ANCHOR TO BE  
INSTALLED NO MORE  
THAN 2½" ABOVE  
GRADE

2-1/2" X 2-1/2"  
X 12 GA. @ 12"  
LONG SLEEVE

FILLET WELD  
ALL AROUND

2-1/4" X 2-1/4"  
X 12 GA. @ 30"  
LONG STEEL TUBE

TWO CORNER BOLTS  
SHALL BE USED TO  
SECURE POST TO  
SLEEVE

Ø7/16" HOLES  
(4 SIDES)

FILLET WELD  
ALL AROUND

**NOTE:**

POST ANCHOR SHALL BE 30"  
ANCHOR-MATE W/12" SLEEVE,  
ULTIMATE HIGHWAY PRODUCTS,  
INC. OR APPROVED EQUAL.

EXPLODED VIEW

SIDE VIEW

**NOTES:**



1. ANY HOLES BELOW TOP TWO HOLES SHALL BE COVERED WITH DUCT TAPE.
2. SECURE POST TO ANCHOR USING CORNER BOLT, SAMPLE PICTURED TO LEFT.



**CITY OF CLOVIS**

DWG NO.

**ST-15A**

**STREET SIGN POST ANCHOR**

REF.:  
STD. SPECIFICATIONS  
SECTION 84

APPROVED BY:

NO.

REVISED

BY

APPROVALS

SCALE: NTS

CITY ENGINEER *[Signature]*

DATE: 6/6/19

05-20-09

BGJ

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01-14-11

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*[Signature]*

09-28-12

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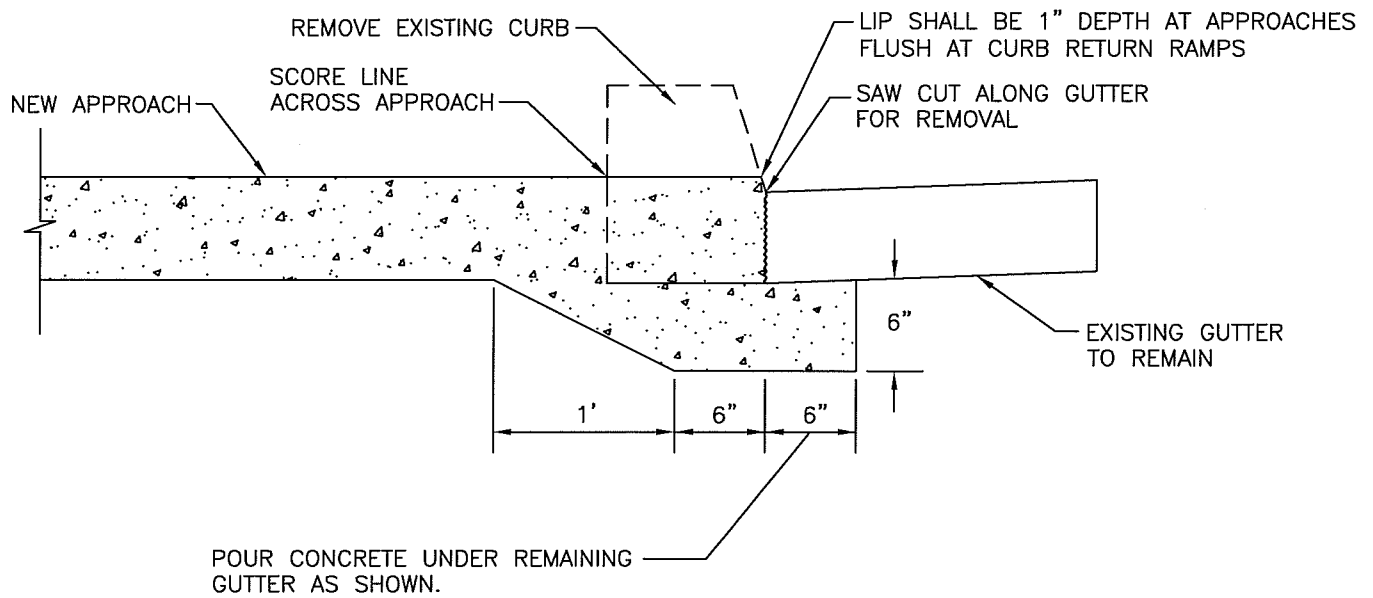
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03-20-19

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DRAWN BY: JA

SHEET 1 OF 1



# CITY OF CLOVIS

DWG NO.  
**ST-16**

## EXISTING CURB REMOVAL FOR NEW APPROACHES/CURB RETURN RAMPS

REF.:  
STD. SPECIFICATIONS  
SECTION 73

APPROVED BY:

NO.

REVISED

BY

APPROVALS

SCALE: NTS

CITY ENGINEER

DATE:

05-15-09

01-13-11

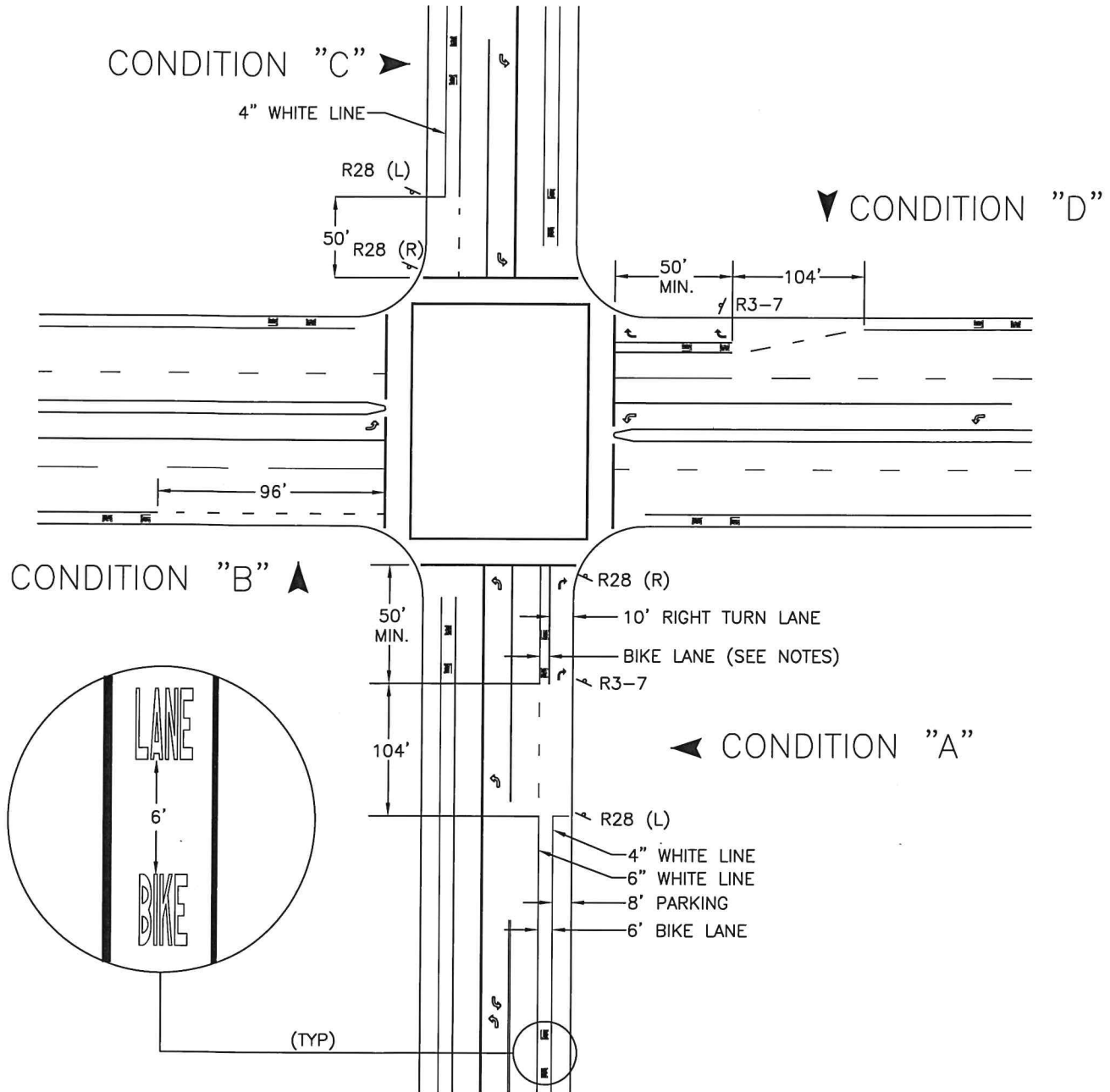
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BGJ

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DRU  
PUD

DRAWN BY: JA

SHEET 1 OF 1



NOTES:

CONDITION "A" = 6' BIKE LANE WITH 8' PARKING TRANSITIONS TO 10' RIGHT TURN LANE AND 4' BIKE LANE (40 MPH AND UNDER) OR 6' BIKE LANE (45 MPH AND OVER).

CONDITION "B" = 5' BIKE LANE WITH NO RIGHT TURN LANE

CONDITION "C" = 8' PARKING AND 6' BIKE LANE EXTENDED TO INTERSECTION

CONDITION "D" = 5' BIKE LANE TRANSITIONS TO RIGHT TURN LANE



# CITY OF CLOVIS

DWG NO.

ST-17

## INTERSECTION BIKE LANE STRIPING

REF.: STD. SPECIFICATIONS  
SECTION 84

APPROVED BY:

*[Signature]*

CITY ENGINEER

DATE: 4/5/21

NO.

REVISED

BY

APPROVALS

SCALE: NTS

06-19-09

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06-10-15

08-13-20

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*[Signature]*

*[Signature]*

DRAWN BY: JA

SHEET 1 OF 1

CENTER AND LANELINE STRIPING SHALL BE SOLID FOR 50' APPROACHING INTERSECTION SECTION EXCEPT WHERE STRIPED THROUGH INTERSECTION.

STRIPING OPPOSITE T-INTERSECTING STREET TO BE STRIPED THROUGH INTERSECTION.

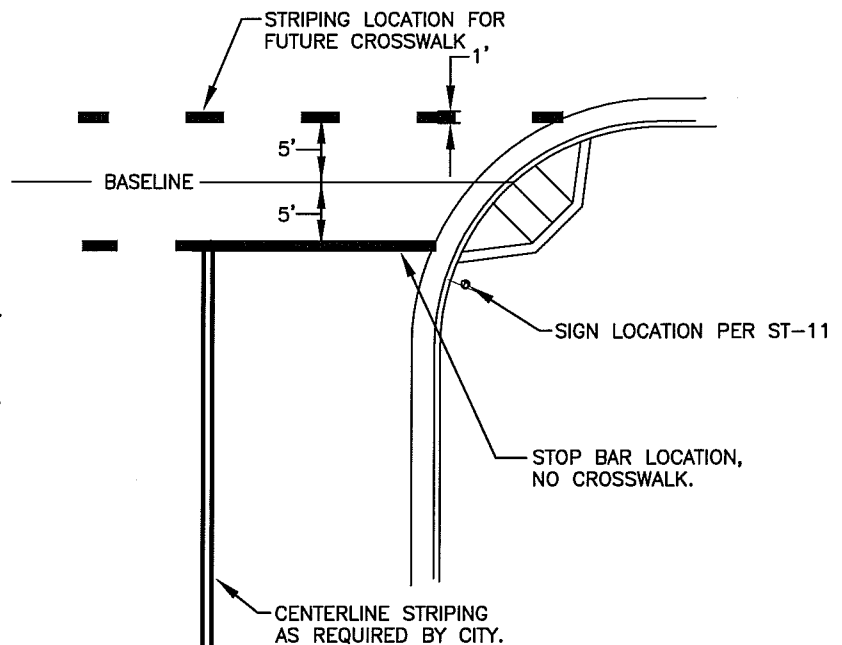
BASELINES FOR STRIPING LAYOUT TO PROJECT FROM CENTER OF RETURNS.

BIKE LANE STRIPING, SEE STANDARD DRAWING FOR INTERSECTION BIKE LANE STRIPING.

BIKE LANE STRIPING TO BEGIN AND END AT CURB RETURN OR CROSSWALK.

#### NOTES:

1. CROSSWALKS ARE SHOWN FOR LOCATION ONLY AND MAY OR MAY NOT BE REQUIRED BY THE CITY.
2. STRIPING AT SIGNALIZED OR FUTURE SIGNALIZED INTERSECTIONS SHOULD BE LAID OUT PER TS-7 AND TS-7A.
3. BASELINES FOR STRIPING LAYOUT SHALL PROJECT TO ULTIMATE CURB RETURN LOCATIONS, UNLESS OTHERWISE DIRECTED.
4. ALL STRIPING TO BE CAT-TRACKED, WITH PLACEMENT APPROVED BY CITY PRIOR TO FINAL PAINT APPLICATION.
5. TEMPLATES FOR MARKINGS, ARROWS, ETC. TO BE BORROWED FROM THE CITY STREET MAINT. SECTION, 324-2600, AND RETURNED IN GOOD AND CLEAN CONDITION.
6. UNLESS OTHERWISE SHOWN ON THIS DRAWING, ALL STRIPING SHALL CONFORM TO CALTRANS STANDARD DRAWINGS AND SPECS.



# CITY OF CLOVIS

## INTERSECTION STRIPING AT NON-SIGNALIZED INTERSECTIONS

DWG NO.

**ST-18**

REF.: STD. SPECIFICATIONS  
SECTION 84

APPROVED BY:

NO.

REVISED

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APPROVALS

SCALE: NTS

CITY ENGINEER

DATE:

7/29/11

06-16-09

01-14-11

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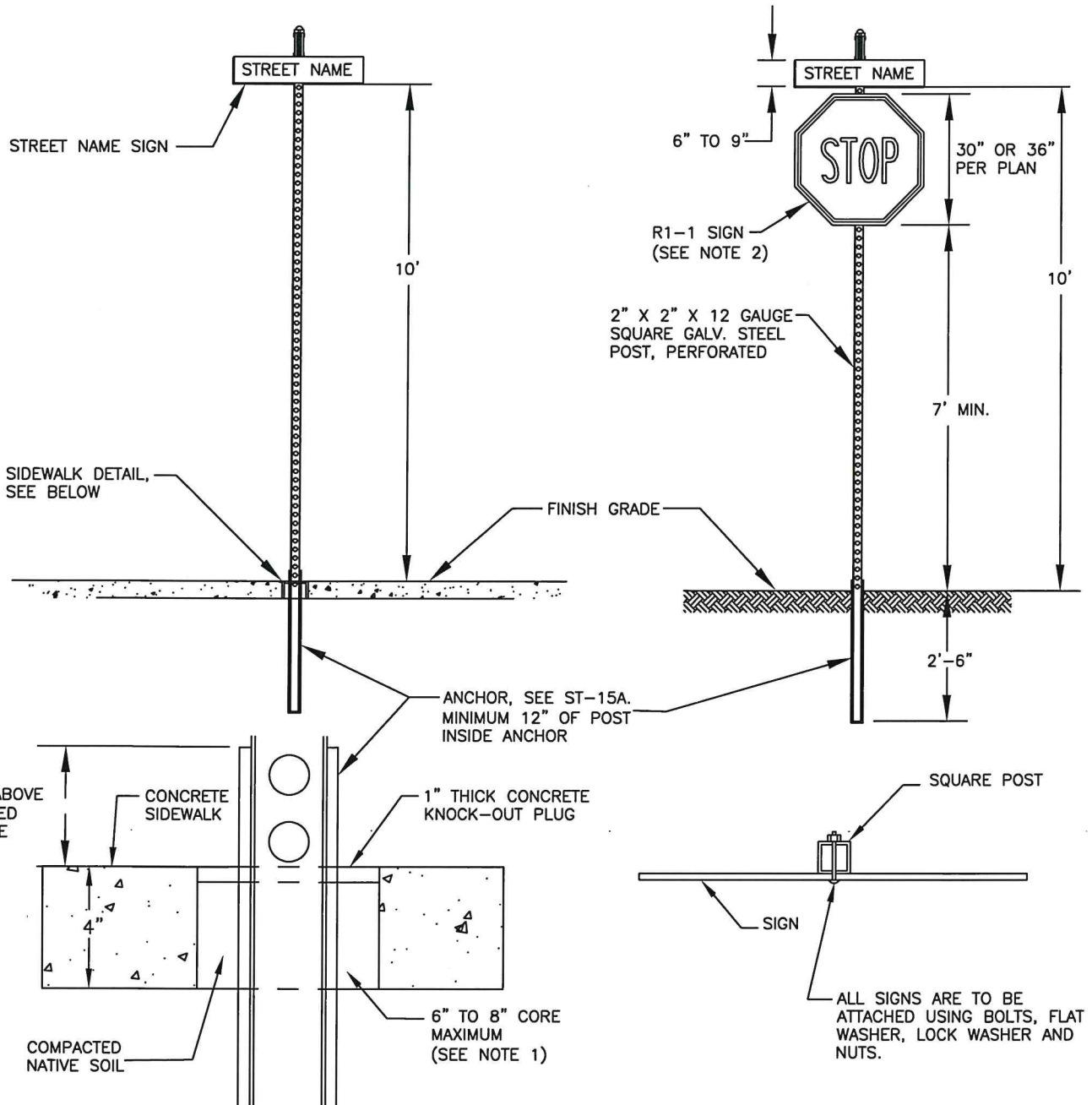
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DRAWN BY: JA

SHEET 1 OF 1





#### NOTES:

1. SIGN LOCATION TO BE DETERMINED BY THE CITY ENGINEER.
2. R1-1 STOP SIGN TO BE 0.08\"
3. SQUARE POST AND ANCHOR FOR ALL INSTALLATIONS.
4. SEE CITY STD. DRAWING ST-11 FOR SIGN PLACEMENT.
5. SECURE POST TO ANCHOR USING TWO CORNER BOLTS. SEE CITY STD. DRAWING ST-15A.
6. ALL STREET NAME SIGNS TO BE 10' FROM GRADE.



# CITY OF CLOVIS

## STREET NAME & STOP SIGN INSTALLATION

DWG NO.

**ST-19**

REF.:  
STD. SPECIFICATIONS  
SECTION 84

APPROVED BY:

NO.

REVISED

BY

APPROVALS

SCALE: NTS

CITY ENGINEER

DATE: 15:32:00-08'00'

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05-21-09

03-20-19

06-05-20

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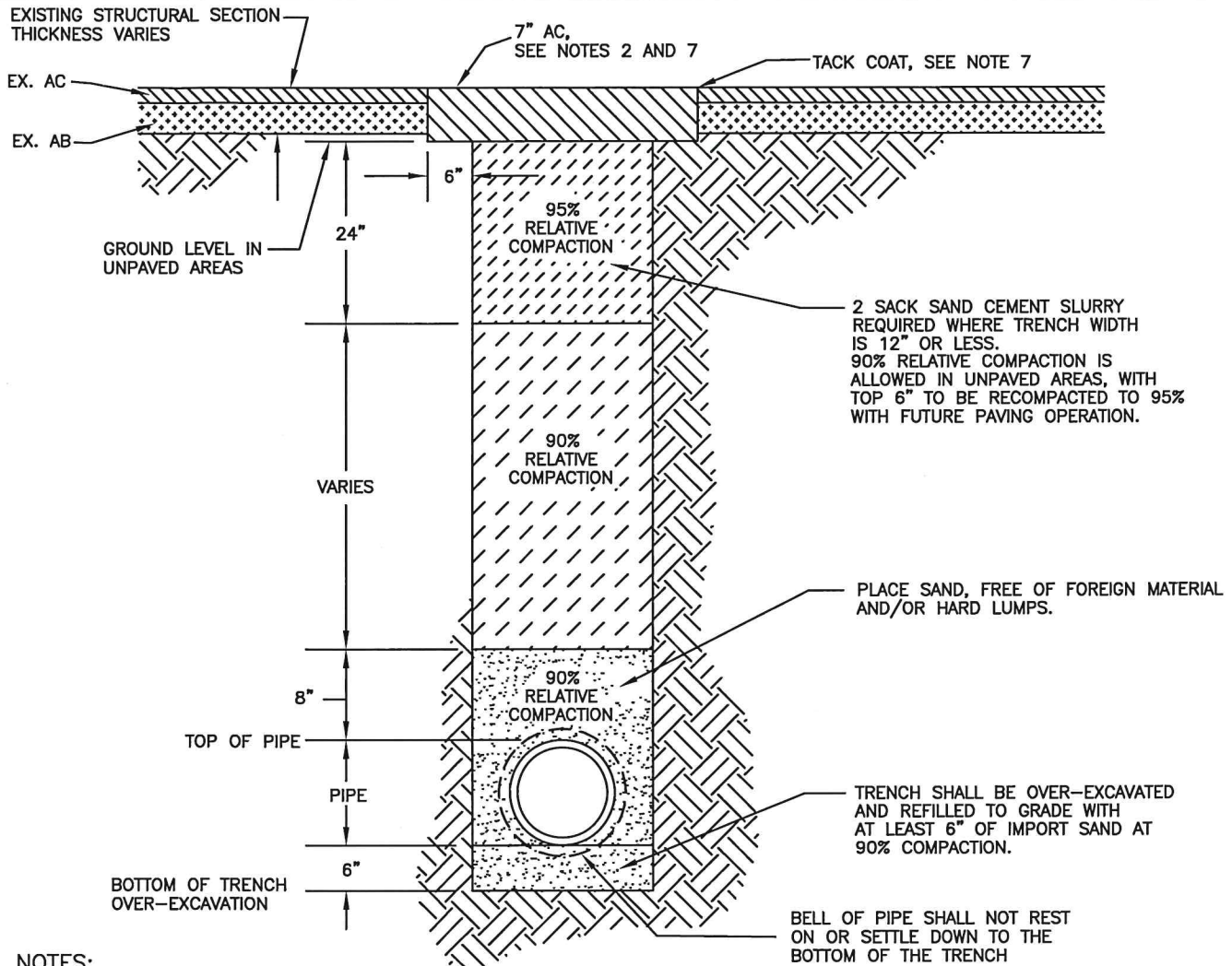
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DRAWN BY: JA

SHEET 1 OF 1



#### NOTES:

1. TEMPORARY PAVEMENT RESURFACING SHALL CONSIST OF A MINIMUM OF 4" COLD MIX AND SHALL BE REQUIRED WHENEVER THE STREET IS TEMPORARILY OPENED TO TRAFFIC. THE TEMPORARY PAVEMENT SHALL BE MAINTAINED PROPERLY BY THE CONTRACTOR FOR SAFETY. IF PERMANENT PAVEMENT IS NOT SCHEDULED TO BE INSTALLED WITHIN 30 DAYS OF TRENCHING, HMA SHALL BE REQUIRED AS TEMPORARY PAVEMENT. ALL TEMPORARY MATERIAL SHALL BE COMPLETELY REMOVED PRIOR TO FINAL PAVING.
2. PERMANENT RESURFACING OF PAVED AREAS SHALL CONSIST OF 7" A.C. PAVING OR MATCH EXISTING PAVEMENT SECTION IF PAVEMENT CUT IS WIDER THAN 6' UNLESS SPECIFIED OTHERWISE. EXISTING PAVEMENT CUT EDGES SHALL BE SAW-CUT AND TACK COAT SHALL BE APPLIED PRIOR TO FINAL PAVING.
3. PAVEMENT CUT RESURFACING OF STRUCTURAL SECTIONS OTHER THAN PERMANENTLY PAVED OR UNPAVED AREAS SHALL BE DETERMINED BY THE CITY ENGINEER.
4. ALL BACKFILL WILL BE TESTED FOR OPTIMUM MOISTURE. TO BE CONSIDERED A PASSING TEST, THE IN-PLACE MATERIAL MUST BE WITHIN 2% (ABOVE OR BELOW) THE OPTIMUM MOISTURE.
5. NO JETTING OF BACKFILL WILL BE ALLOWED. BACKFILL IS TO BE PLACED IN MAXIMUM 18" LIFTS, THEN COMPACTED.
6. FOR PAVEMENT EXCAVATION WITH DIAMETER 9" OR LESS OR MAXIMUM DIMENSION IN ANY DIRECTION OF 9" OR LESS, OR WHERE DIRECTED BY THE ENGINEER, BACKFILL SHALL COMPLY WITH ST-21, BACKFILL AND RESURFACING - AC EXCAVATION.
7. UNLESS OTHERWISE DIRECTED BY THE CITY ENGINEER, ALL CUTS MADE IN PAVEMENT GENERALLY 5 YEARS AND YOUNGER SHALL BE TREATED WITH THE INFRARED PAVEMENT REPAIR PROCESS A MINIMUM OF 6" ON EACH SIDE OF SAWCUT, PER SECTION 19-3.05 OF THE CITY STANDARD SPECIFICATIONS. FOR PAVEMENT CUTS LESS THAN 2' IN WIDTH, THE INFRARED PAVEMENT REPAIR SHALL BE APPLIED TO THE ENTIRE AREA. WHEN INFRARED PAVEMENT REPAIR IS NOT REQUIRED, A CRACK SEALANT SHALL BE APPLIED ALONG THE PAVEMENT CUTS.
8. IT IS THE INTENT AND PREFERENCE THAT SAND BE USED TO ENSURE PIPE PROTECTION AND PROPER COMPACTION AROUND THE PIPE. WHERE SUITABLE GRANULAR MATERIAL IS ENCOUNTERED IN THE FIELD, SUCH ALTERNATIVE BACKFILL MATERIAL MAY BE CONSIDERED ON A CASE BY CASE BASIS, SUBJECT TO APPROVAL BY THE CITY ENGINEER.



# CITY OF CLOVIS

## BACKFILL AND RESURFACING - TRENCH

DWG NO.

ST-20

REF.:  
STD. SPECIFICATIONS  
SECTION 19

APPROVED BY:

*[Signature]*

CITY ENGINEER

DATE: 1/6/2020

NO.

REVISED

BY

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01-13-11

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DRU

03-11-19

CGV

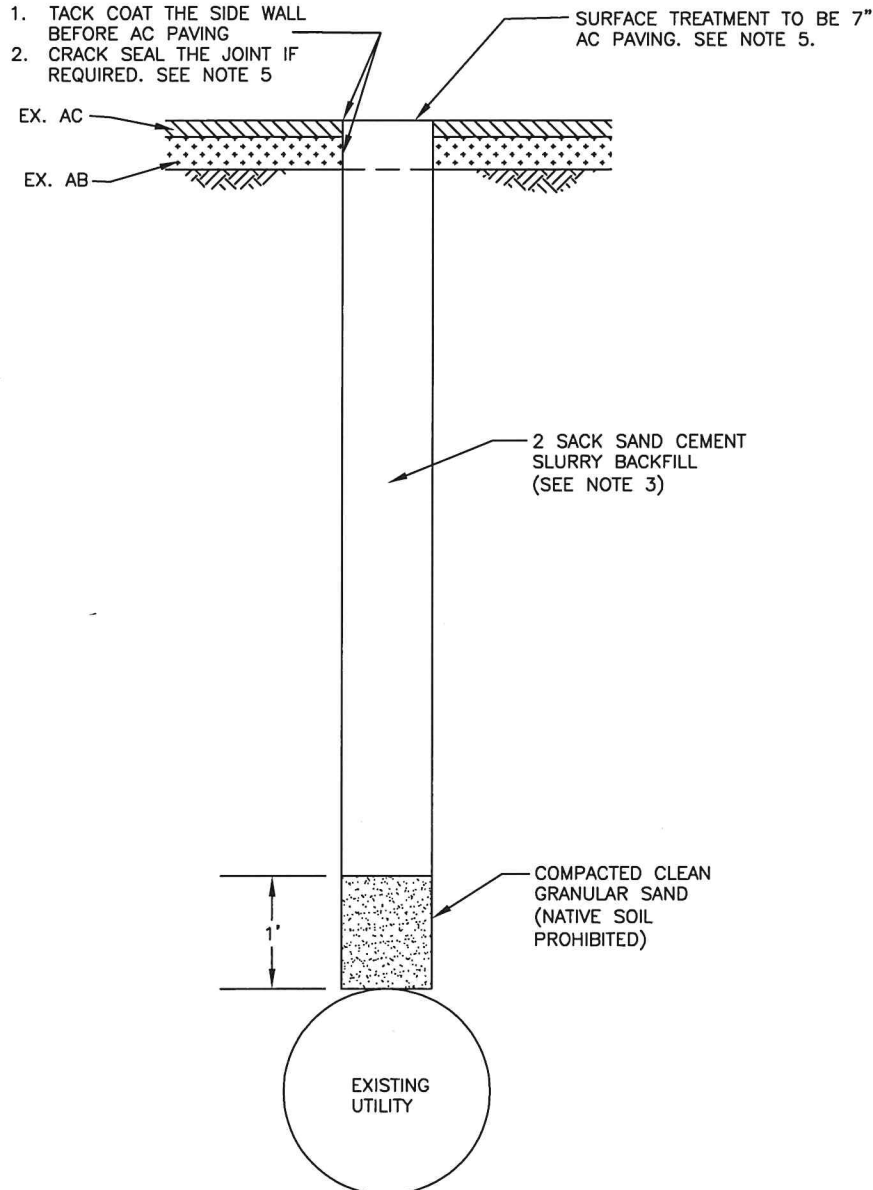
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SCALE: NTS

DRAWN BY: JA

SHEET 1 OF 1





**NOTES:**

1. THIS BACKFILL METHOD IS REQUIRED FOR AC EXCAVATION OF 9" DIAMETER OR LESS, OR MAXIMUM DIMENSION IN ANY DIRECTION OF 9" OR LESS, OR ANY AC EXCAVATION DIRECTED BY THE ENGINEER TO BE BACKFILLED BY THIS METHOD. FOR INVESTIGATIVE WORK LESS THAN 4" IN DIAMETER, BLACK CONCRETE SLURRY BACKFILL TO FINISH GRADE IS ACCEPTABLE WITH CITY ENGINEER APPROVAL.
2. AC EXCAVATION WITH DIMENSIONS GREATER THAN 9" SHALL BE BACKFILLED PER THE STANDARD DRAWING ST-20 FOR TRENCH BACKFILL AND SURFACE RESTORATION.
3. SLURRY CEMENT BACKFILL PLACEMENT SHALL CONFORM TO SECTION 19-3.03F OF THE STATE STANDARD SPECIFICATIONS.
4. TEMPORARY RESURFACING SHALL CONSIST OF A MINIMUM OF 4" COLD MIX AND SHALL BE REQUIRED WHENEVER THE STREET IS TEMPORARILY OPENED TO TRAFFIC. PERMANENT PAVEMENT RESURFACING SHALL BE INSTALLED WITHIN 7 DAYS AFTER THE INITIAL PAVEMENT CUT. ALL TEMPORARY MATERIAL SHALL BE COMPLETELY REMOVED PRIOR TO FINAL PAVING.
5. UNLESS OTHERWISE DIRECTED BY THE CITY ENGINEER, ALL CUTS MADE IN PAVEMENT GENERALLY 5 YEARS AND YOUNGER SHALL BE TREATED WITH THE INFRARED PAVEMENT REPAIR PROCESS A MINIMUM OF 6" ON EACH SIDE OF SAWCUT, PER SECTION 19-3.05 OF THE CITY STANDARD SPECIFICATIONS. WHEN INFRARED PAVEMENT REPAIR IS NOT REQUIRED, A CRACK SEALANT SHALL BE APPLIED ALONG THE PAVEMENT CUTS.



# CITY OF CLOVIS

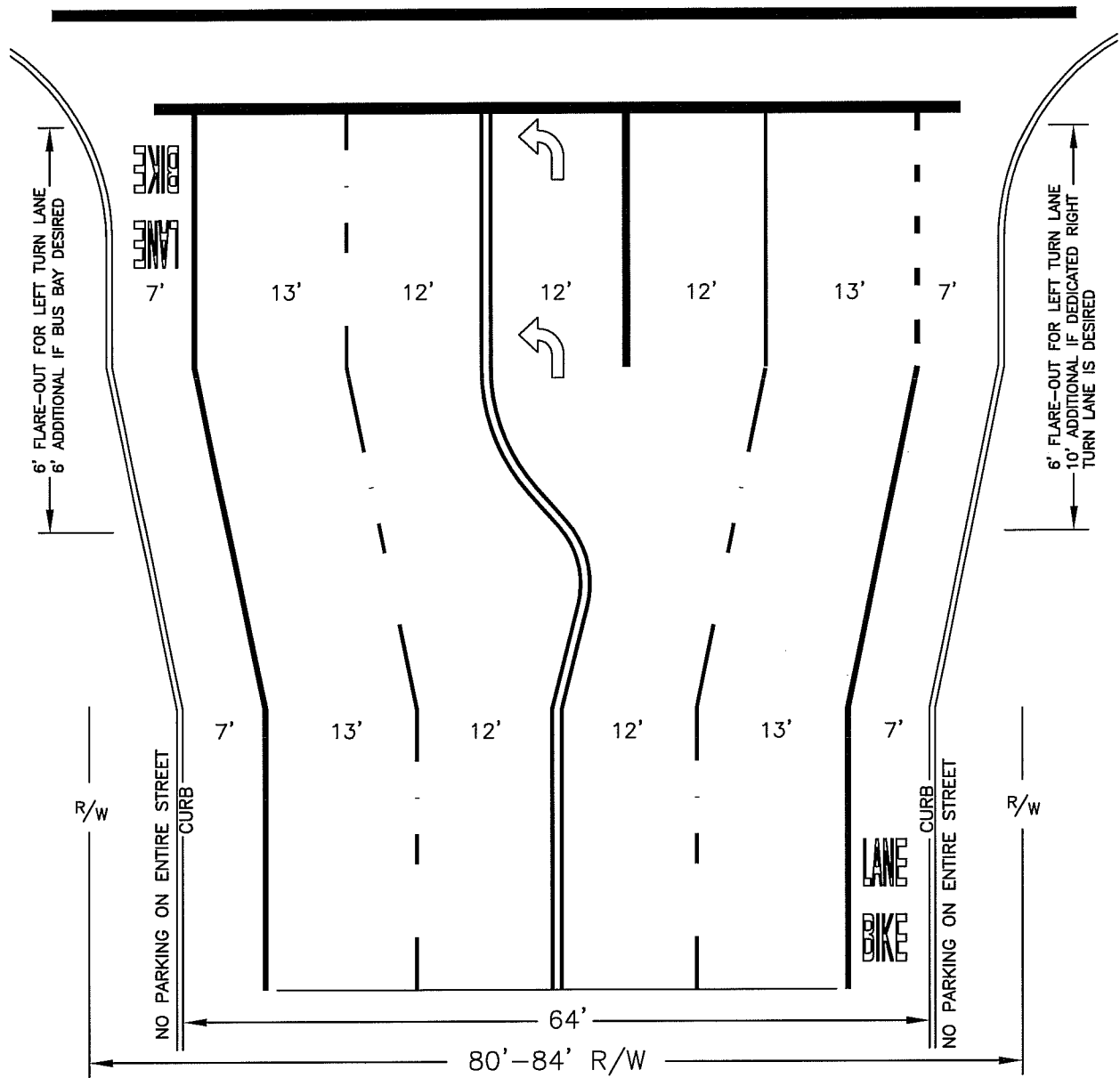
DWG NO.

**ST-21**

## BACKFILL AND RESURFACING - AC EXCAVATION

REF.:  
STD. SPECIFICATIONS  
SECTION 8-15

APPROVED BY:  CITY ENGINEER DATE: 1/6/2020	NO.	REVISED	BY	APPROVALS CM DRU PUD	SCALE: NTS
		05-12-09	BGJ		DRAWN BY: JA
		02-15-11	BGJ		
	3	03-27-19	CGV		SHEET 1 OF 1



**NOTE:**

THIS DRAWING IS SCHEMATIC AND A DESIGN AID ONLY. SPACING AND SIZING OF ARROWS, LEGENDS, STRIPING, LENGTH OF LEFT AND RIGHT TURN LANES, AND ALL OTHER DESIGN DETAILS SHALL CONFORM TO THE LATEST EDITION OF CALMUTCD.



# CITY OF CLOVIS

## GEOMETRICS, 4 LANE COLLECTOR

DWG NO.  
**ST-22**

REF.: STD. SPECIFICATIONS  
SECTION 84

APPROVED BY:

NO.

REVISED

BY

APPROVALS

SCALE: NTS

CITY ENGINEER

DATE:

*[Signature]*  
4/29/11

06-17-09

BGJ

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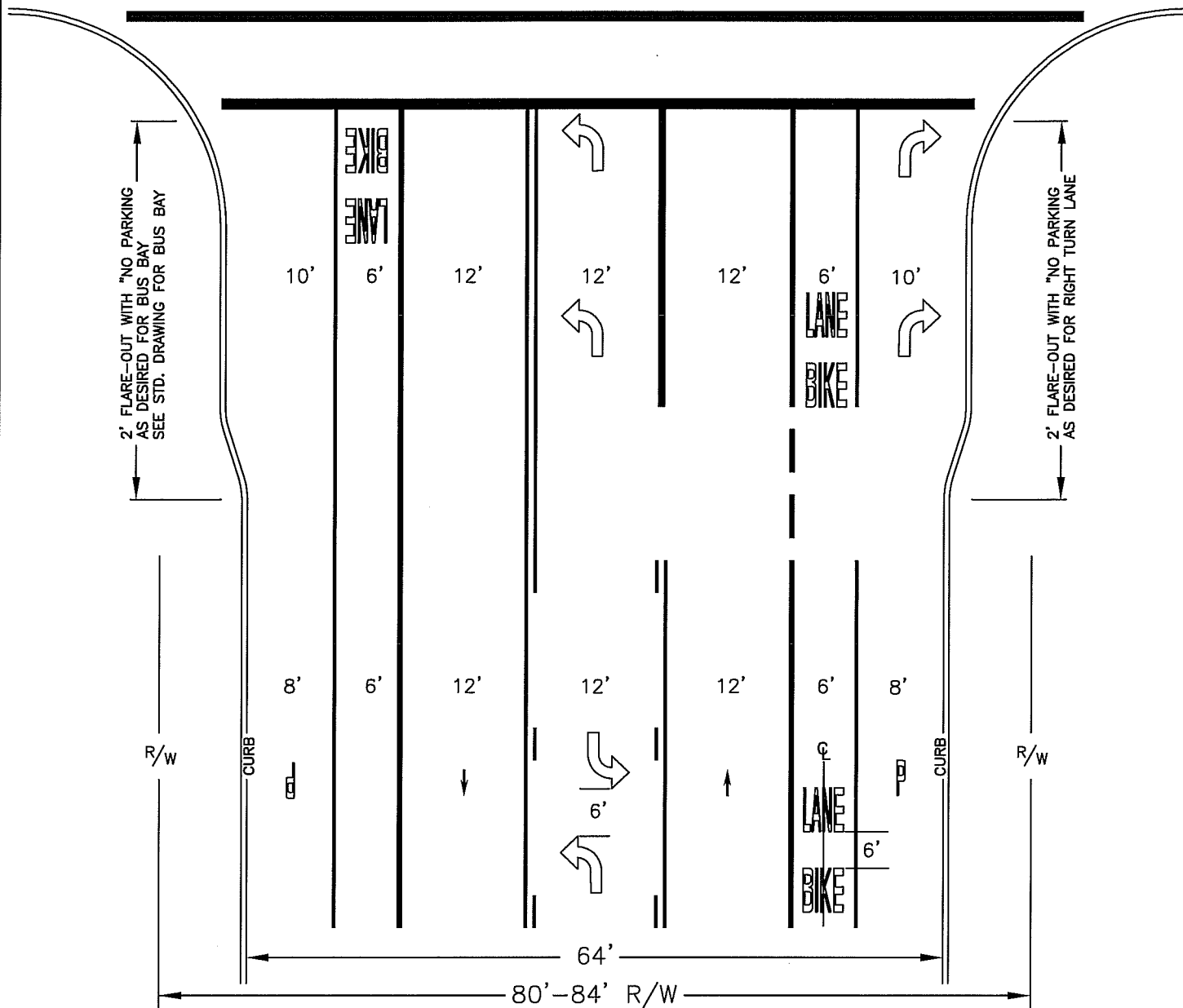
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DRAWN BY: JA

SHEET 1 OF 1



**NOTE:**

THIS DRAWING IS SCHEMATIC AND A DESIGN AID ONLY. SPACING AND SIZING OF ARROWS, LEGENDS, STRIPING, LENGTH OF LEFT AND RIGHT TURN LANES, AND ALL OTHER DESIGN DETAILS SHALL CONFORM TO THE LATEST EDITION OF CALMUTCD.



# CITY OF CLOVIS

GEOMETRICS, COLLECTOR  
W/ 2-WAY LEFT TURN LANE

DWG NO.  
**ST-22A**

REF.: STD. SPECIFICATIONS  
SECTION 84

APPROVED BY:

NO.

REVISED

BY

APPROVALS

SCALE: NTS

CITY ENGINEER

DATE:

7/19/11

06-17-09

01-14-11

BGJ

BGJ

CM

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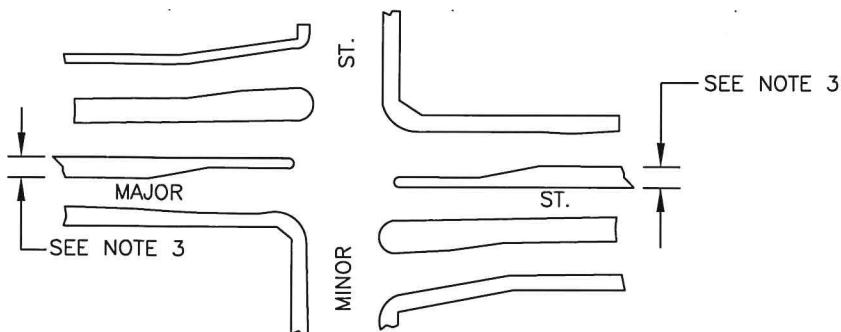
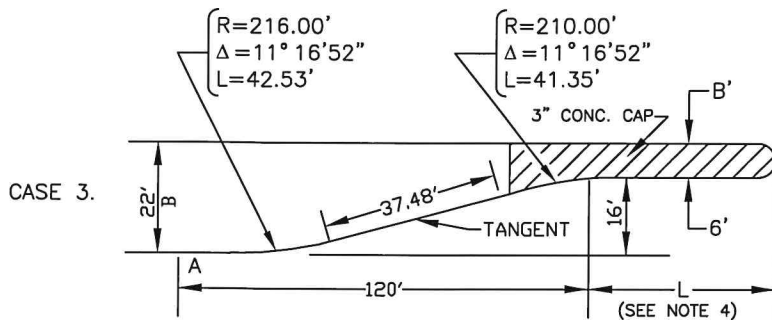
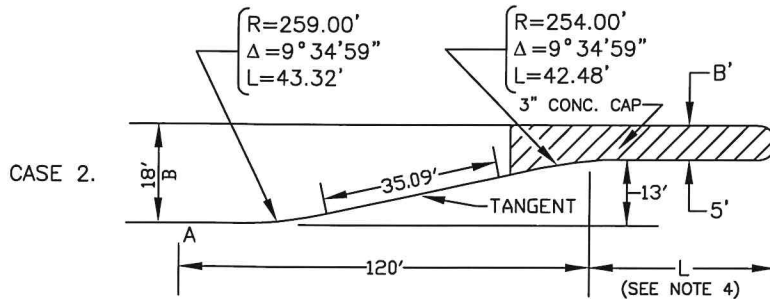
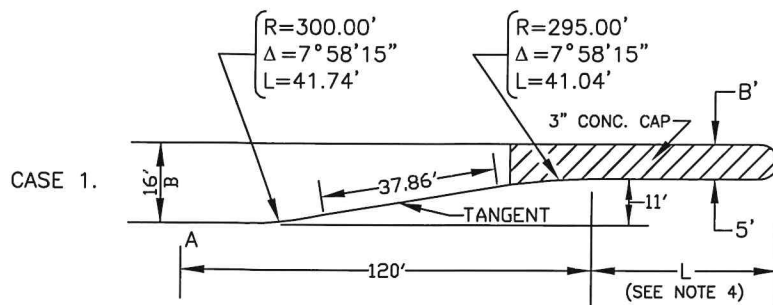
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DRAWN BY: JA

SHEET 1 OF 1

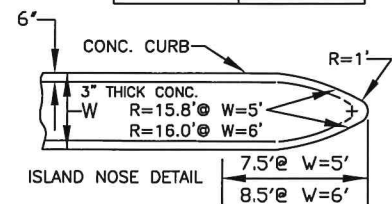




CURVE TABLE	
DIST. FROM POINT 'A'	OFFSET B-B'=11'
0'	0.00'
10'	0.17'
20'	0.67'
30'	1.50'
40'	2.68'
41.60'	2.90'
50'	4.08'
60'	5.48'
70'	6.88'
79.09'	8.15'
80'	8.28'
90'	9.47'
100'	10.32'
110'	10.83'
120'	11.00'

CURVE TABLE	
DIST. FROM POINT 'A'	OFFSET B-B'=13'
0'	0.00'
10'	0.19'
20'	0.77'
30'	1.74'
40'	3.08'
43.12'	3.61'
50'	4.77'
60'	6.46'
70'	8.16'
77.71'	9.46'
80'	9.86'
90'	11.23'
100'	12.21'
110'	12.80'
120'	13.00'

CURVE TABLE	
DIST. FROM POINT 'A'	OFFSET B-B'=16'
0'	0.00'
10'	0.23'
20'	0.93'
30'	2.08'
40'	3.69'
42.25'	4.17'
50'	5.80'
60'	7.90'
70'	10.00'
78.92'	11.92'
80'	12.20'
90'	13.86'
100'	15.05'
110'	15.76'
120'	16.00'



#### NOTES:

1. END 3" THICK CLASS 3 CONCRETE CAP WHERE CURBS ARE 9' APART, AS MEASURED FROM INSIDE BACK OF CURBS. CERTAIN LOCATIONS MAY REQUIRE STAMPED CONCRETE.
2. CONCRETE GUTTER TO BE USED WHERE REQUIRED.
3. WIDTH DETERMINED BY SELECTED CASE 1, 2 OR 3.
4. DISTANCE "L" TO BE DETERMINED BY THE ENGINEER.



# CITY OF CLOVIS

## MEDIAN ISLAND INTERSECTION LEFT TURN POCKETS

DWG NO.  
**ST-23**

N.A.

APPROVED BY:

CITY ENGINEER

DATE: 4/5/21

NO.

REVISED

BY

APPROVALS

SCALE: NTS

NO.

REVISED

BY

APPROVALS

DRAWN BY: JA

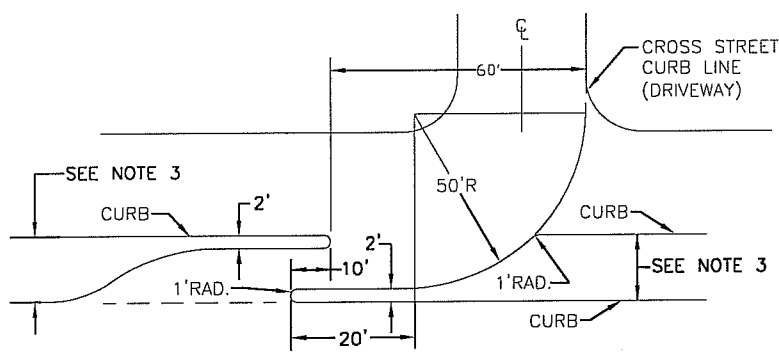
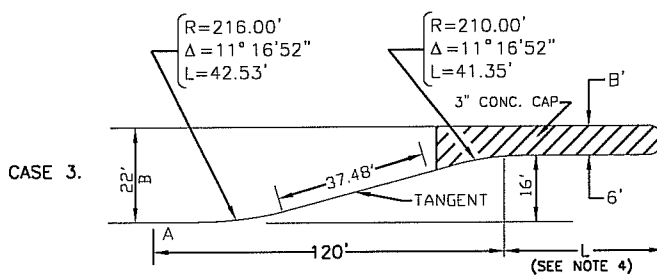
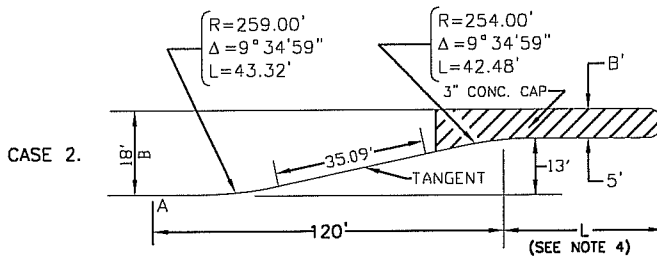
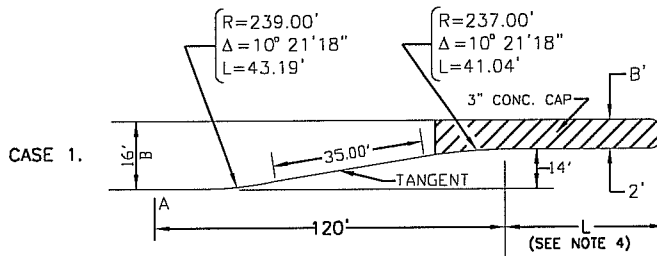
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APPROVALS

SHEET 1 OF 1



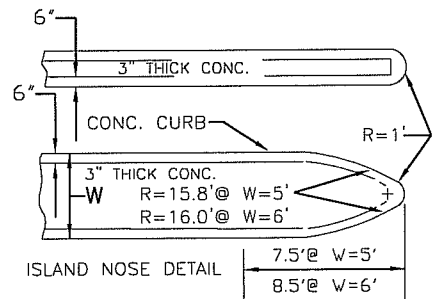
**NOTES:**

1. END 3" THICK CLASS 3 CONCRETE CAP WHERE CURBS ARE 9' APART, AS MEASURED FROM INSIDE BACK OF CURBS. CERTAIN LOCATIONS MAY REQUIRE STAMPED CONCRETE.
2. CONCRETE GUTTER TO BE USED WHERE REQUIRED.
3. WIDTH DETERMINED BY SELECTED CASE 1, 2 OR 3.
4. DISTANCE "L" TO BE DETERMINED BY THE ENGINEER.

CURVE TABLE	
DIST. FROM POINT 'A'	OFFSET B-B'=14'
0'	0.00'
10'	0.21'
20'	0.84'
30'	1.88'
40'	3.34'
42.95'	3.89'
50'	5.17'
60'	6.98'
70'	8.80'
77.40'	10.14'
80'	10.63'
90'	12.10'
100'	13.15'
110'	13.79'
120'	14.00'

CURVE TABLE	
DIST. FROM POINT 'A'	OFFSET B-B'=13'
0'	0.00'
10'	0.19'
20'	0.77'
30'	1.74'
40'	3.08'
43.12'	3.61'
50'	4.77'
60'	6.46'
70'	8.16'
77.71'	9.46'
80'	9.86'
90'	11.23'
100'	12.21'
110'	12.80'
120'	13.00'

CURVE TABLE	
DIST. FROM POINT 'A'	OFFSET B-B'=16'
0'	0.00'
10'	0.23'
20'	0.93'
30'	2.08'
40'	3.69'
42.25'	4.17'
50'	5.80'
60'	7.90'
70'	10.00'
78.92'	11.92'
80'	12.20'
90'	13.86'
100'	15.05'
110'	15.76'
120'	16.00'



# CITY OF CLOVIS

## MEDIAN ISLAND MID-BLOCK LEFT TURN POCKET

DWG NO.

**ST-24**

N.A.

APPROVED BY:

NO.

REVISED

BY

APPROVALS

SCALE: NTS

CITY ENGINEER

DATE:

NO.

REVISED

BY

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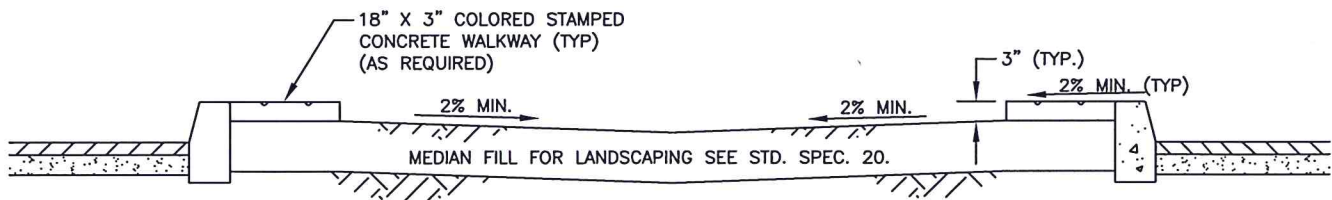
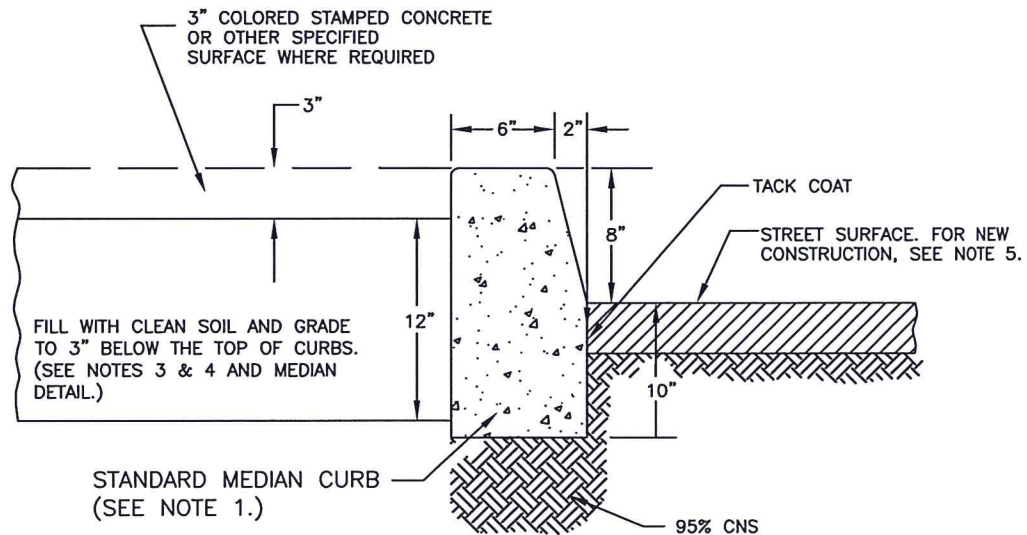
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SHEET 1 OF 1

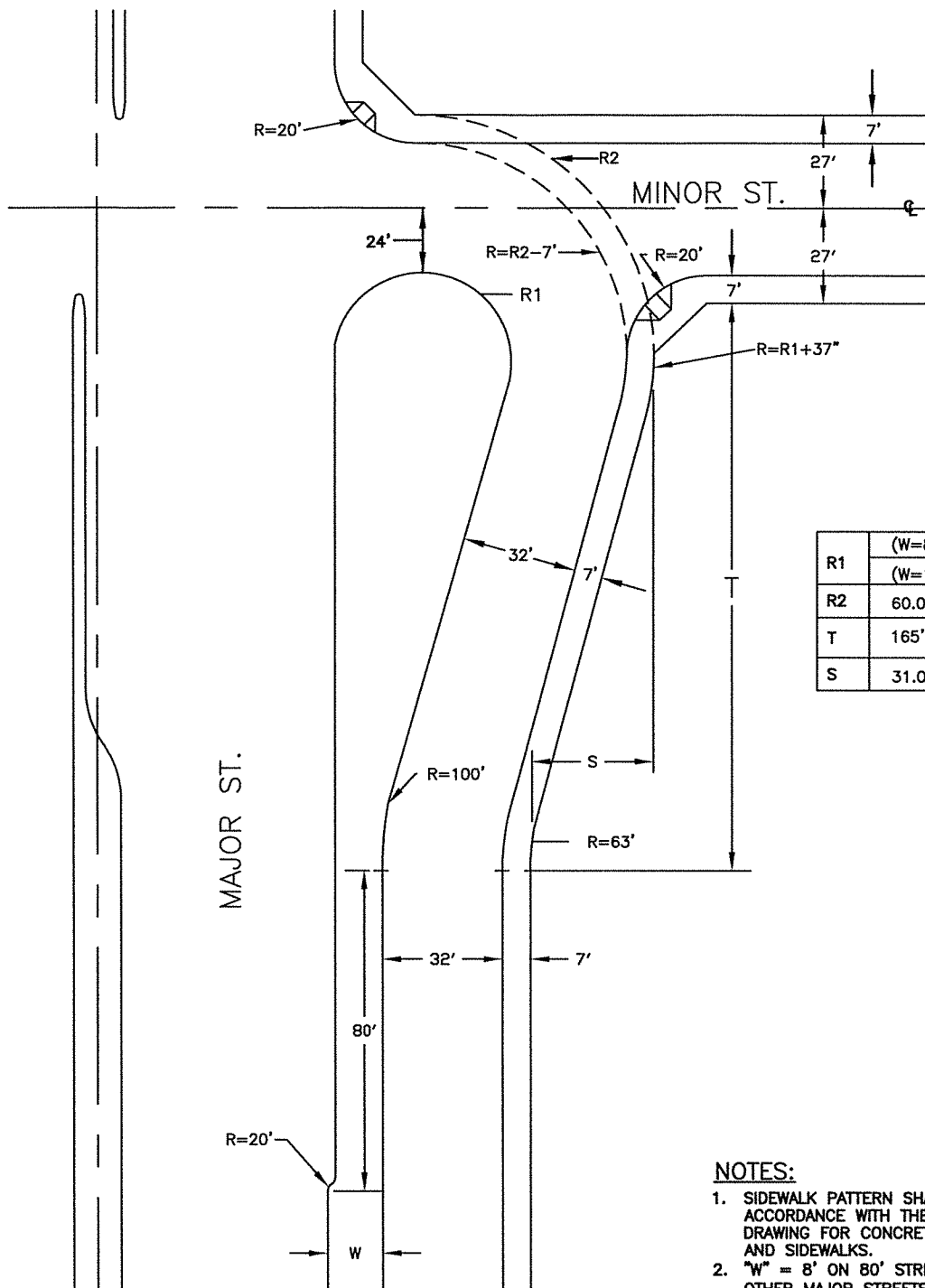


MEDIAN DETAIL

NOTES:

1. FORMED CURB SHALL BE CLASS 3 CONCRETE; EXTRUDED CURBS SHALL BE CLASS 2.
2. CURBS SHALL BE BROOM FINISH.
3. COMPACT MEDIAN FILL MATERIAL TO 90% UNDER SURFACES TO BE PAVED.
4. MEDIAN FILL MATERIAL MAY VARY DEPENDING ON LANDSCAPING REQUIREMENTS.
5. FILL INSIDE MEDIAN BEFORE PLACING AND/OR COMPACTING MATERIAL IN THE TRAVELED WAY.
6. ALL COLORED STAMPED CONCRETE SHALL BE SEALED WITH TWO COATS OF "CLEAR SEAL 100", OR APPROVED EQUAL.

	<h1 style="text-align: center;">CITY OF CLOVIS</h1> <h2 style="text-align: center;">MEDIAN ISLAND CURB DETAILS</h2> <p style="text-align: center;">(NEW AC PAVEMENT)</p>					DWG NO. <h2 style="text-align: center;">ST-25</h2>
						REF.: STD. SPECIFICATIONS
APPROVED BY:  CITY ENGINEER <i>[Signature]</i> DATE: <i>6/29/17</i>	NO. _____ _____ _____ _____	REVISED 3-26-12 05-29-12 09-28-12 06-12-17	BY PAA PAA PAA CGV	APPROVALS CM <i>[Signature]</i> DRU <i>[Signature]</i> PUD <i>[Signature]</i>	SCALE: NTS  DRAWN BY: JA  SHEET 1 OF 1	



**NOTES:**

1. SIDEWALK PATTERN SHALL BE IN ACCORDANCE WITH THE STANDARD DRAWING FOR CONCRETE CURB & GUTTER AND SIDEWALKS.
2. "W" = 8' ON 80' STREETS, 10' ON ALL OTHER MAJOR STREETS (84', 100', 106').



# CITY OF CLOVIS

## FRONTAGE ROAD BULB DESIGN

DWG NO.

**ST-26**

N.A.

APPROVED BY:

CITY ENGINEER

DATE:

NO.

REVISED

BY

APPROVALS

SCALE: NTS

06-19-09

BGJ

CM

DRAWN BY: JA

01-14-11

BGJ

DRU

SHEET 1 OF 1

01-30-14

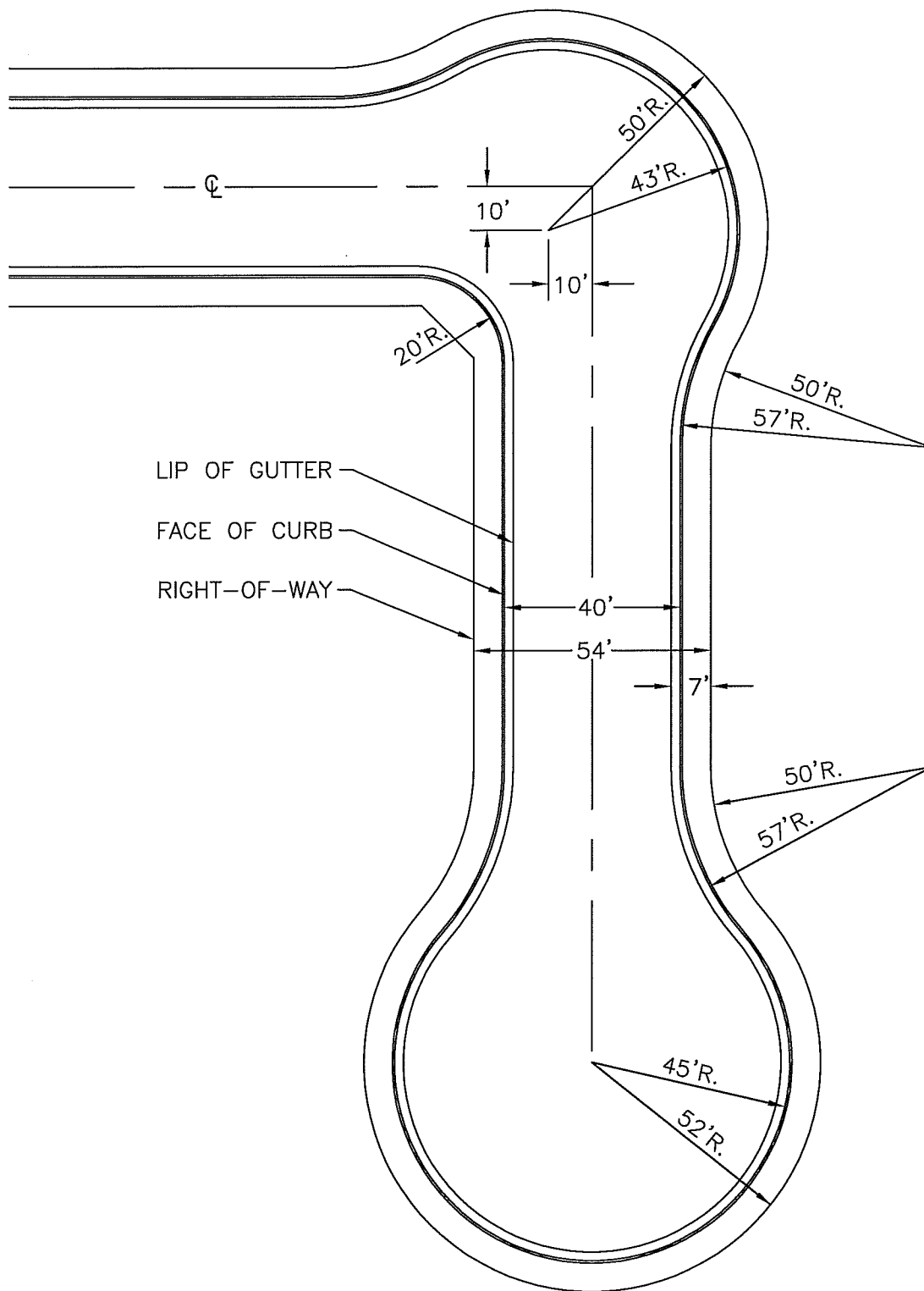
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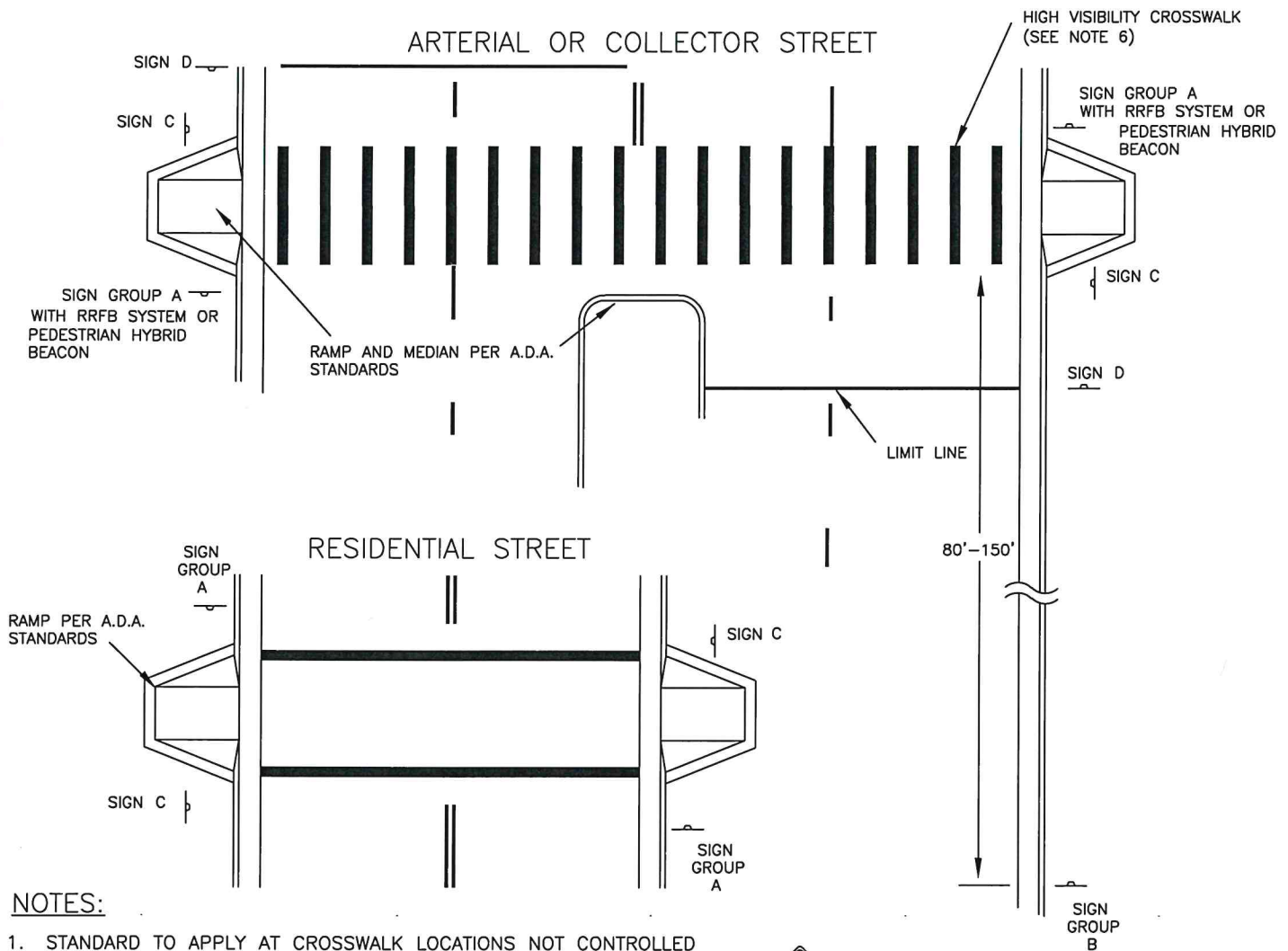
06-11-15

CGV





	<h1 style="margin: 0;">CITY OF CLOVIS</h1>				DWG NO. <h2 style="margin: 0;">ST-27</h2>
	RESIDENTIAL STREET KNUCKLE AND CUL-DE-SAC DESIGN				N.A.
APPROVED BY:  CITY ENGINEER DATE: <i>7/19/11</i>	NO. _____ _____ _____	REVISED 07-09-09 01-14-11	BY BGJ BGJ	APPROVALS CM DRU PUD <i>[Signature]</i>	SCALE: NTS DRAWN BY: JA SHEET 1 OF 1



#### NOTES:

1. STANDARD TO APPLY AT CROSSWALK LOCATIONS NOT CONTROLLED BY STOP SIGNS OR TRAFFIC SIGNALS.
2. SIGN GROUPS A AND B TO BE FLUORESCENT YELLOW-GREEN 0.08" THICK ALUMINUM; FACE SHALL HAVE 3M DG-3 REFLECTIVE SHEETING W/AN APPLIED 3M #1160 OR EQUAL GRAFFITI FILM. SIGN SHALL CONFORM TO CAL MUTCD SPECIFICATIONS. SIGN S1-1 TO BE USED AT SCHOOL CROSSINGS ONLY. SIGN W11-2 TO BE USED AT ALL OTHER LOCATIONS.
3. RECTANGULAR RAPID FLASH BEACON (RRFB) LIGHTING SYSTEM MAY BE SOLAR-POWERED OR CONNECTED TO NEARBY METERED POWER, UPON APPROVAL OF CITY ENGINEER. THE RRFB SHOULD BE MOUNTED ON 2 1/2" X 2 1/2" X 12 GAUGE POST.
4. WITH A PED HYBRID BEACON, ALSO USE R10-23.
5. SIGN GROUP C CONSISTS OF R1-1/W4-4P FOR MID-BLOCK CROSSING.
6. INSTALL HIGH VISIBILITY CROSSWALK USING VIZIGRIP PREFORMED THERMOPLASTIC MATERIAL OR APPROVED EQUAL, WITH 2' STRIPE AND 2' SPACE. ALL CROSSWALK MARKINGS MUST BE WHITE EXCEPT THOSE NEAR SCHOOLS MUST BE YELLOW.
7. YIELD LINES, PED XING OR SCHOOL XING PAVEMENT MARKINGS MAY BE PLACED IN EACH APPROACH LANE TO A MARKED MID-BLOCK CROSSWALK AS DIRECTED BY THE CITY ENGINEER.



W11-2  
W16-7P



S1-1  
W16-7P

SIGN GROUP A  
(SEE NOTE 2)



W11-2  
W16-9P



S1-1  
W16-9P

SIGN GROUP B  
(SEE NOTE 2)

12" x 18" SIGN  
(NON-STANDARD)



SIGN C

48" TO 54" AG



R10-23  
SIGN D

(SEE NOTE 4)



# CITY OF CLOVIS

## MID-BLOCK PEDESTRIAN CROSSING

DWG NO.

ST-28

REF.: STD. SPECIFICATIONS  
SECTION 84

APPROVED BY:

NO.

REVISED

BY

APPROVALS

SCALE: NTS

CITY ENGINEER

2020.12.10

DATE: 15:26:59-08'00'

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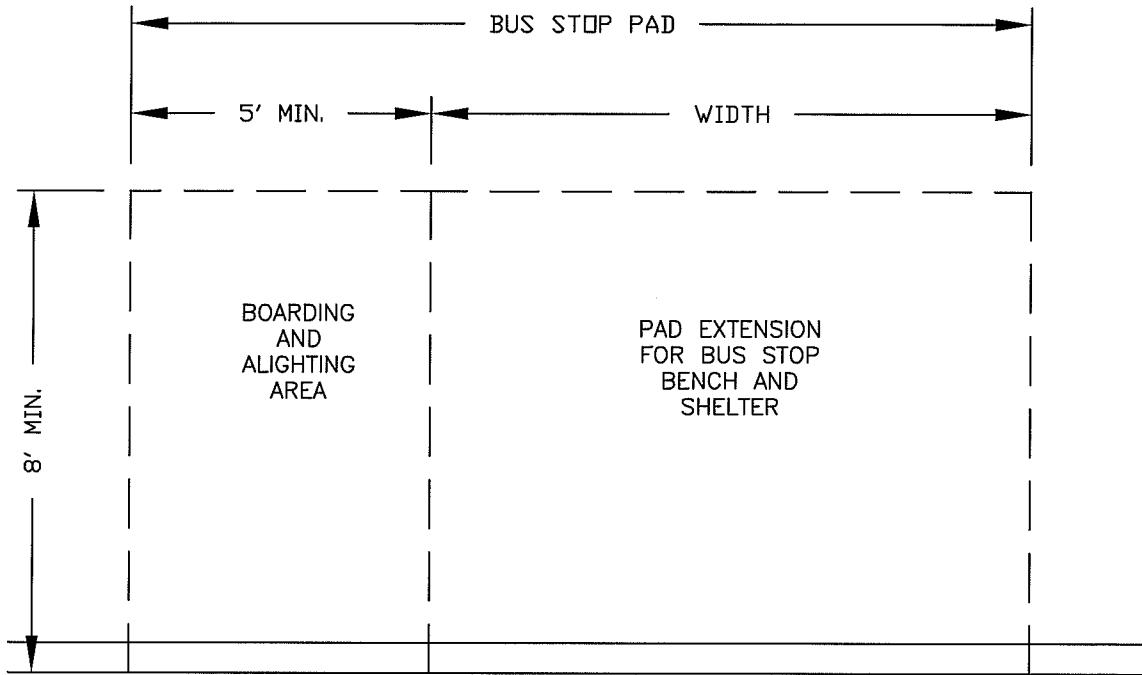
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DRAWN BY: JA

SHEET 1 OF 1

BUS STOP PAD = BOARDING AND ALIGHTING AREA + PAD EXTENSION (WHEN REQUIRED)



DIMENSIONS FOR PAD EXTENSION	
FACILITY	WIDTH
BENCH ONLY	10' MIN.
BUS SHELTER	15' MIN.

CURB FACE OR VEHICLE ROADWAY EDGE

#### NOTES:

1. AN ACCESSIBLE ROUTE SHALL CONNECT THE BOARDING AND ALIGHTING AREA TO ACCESSIBLE FACILITIES AND ELEMENTS, SUCH AS, SIDEWALKS, CLEAR FLOOR SPACE, BENCH, SHELTER, TRASH RECEPTACLE AND TRANSIT TUBE.
2. PERPENDICULAR TO THE ROADWAY, THE SLOPE OF THE BUS STOP PAD SHALL NOT EXCEED 2%.
3. PARALLEL TO THE ROADWAY, THE SLOPE OF THE BUS STOP PAD SHALL BE THE SAME AS THE ROADWAY.
4. THE BOUNDARY BETWEEN THE PEDESTRIAN AREAS (INCLUSIVE OF THE BUS STOP PAD) AND ROADWAY SHALL BE SEPARATED BY A SQUARE CURB SURFACE OR OTHER DETECTABLE WARNING IN ACCORDANCE WITH CURRENT ADA STANDARDS.
5. REFER TO CITY STANDARD ST-5 FOR COMPACTION, CONCRETE THICKNESS, SCORE AND JOINT PATTERN, AND SURFACE FINISH OF THE BUS STOP PAD.
6. FINISHED SURFACE OF THE BUS STOP PAD SHALL BE COMPLETED IN ACCORDANCE WITH CURRENT ADA STANDARDS.
7. A CITY APPROVED BUS STOP SIGN SHALL BE INSTALLED AT THE STOP. ATTACHING SIGN TO A LIGHT POLE IS PREFERRED, OTHERWISE, INSTALL SIGN AS DIRECTED BY CITY ENGINEER.
8. WHEN A BENCH IS PROVIDED, A 30"x48" CLEAR FLOOR SPACE SHALL BE PROVIDED IMMEDIATELY ADJACENT TO THE BENCH FOR WHEELCHAIR ACCESSIBILITY IN ACCORDANCE WITH CURRENT ADA STANDARDS. THE BENCH AND CLEAR FLOOR SPACE SHALL NOT ENCROACH INTO THE BOARDING AND ALIGHTING AREA.
9. WHEN A SHELTER IS PROVIDED, A 30"x48" CLEAR FLOOR SPACE SHALL BE PROVIDED COMPLETELY WITHIN THE BUS STOP SHELTER FOR WHEELCHAIR ACCESSIBILITY IN ACCORDANCE WITH CURRENT ADA STANDARDS.
10. TRANSIT TUBES SHALL BE CONNECTED TO AN ACCESSIBLE ROUTE AND INSTALLED (FOR EXAMPLE CLEAR FLOOR SPACE, REACH RANGE AND OPERATION HEIGHT) IN ACCORDANCE WITH CURRENT ADA STANDARDS. THE OPERATION HEIGHT OF THE TRANSIT TUBE SHALL NOT EXCEED 40" ABOVE FINISH SURFACE OF THE CLEAR FLOOR SPACE.



# CITY OF CLOVIS

DWG NO.

## ST-29

### BUS STOP

REF. CURRENT ADA STANDARDS

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SCALE: NTS

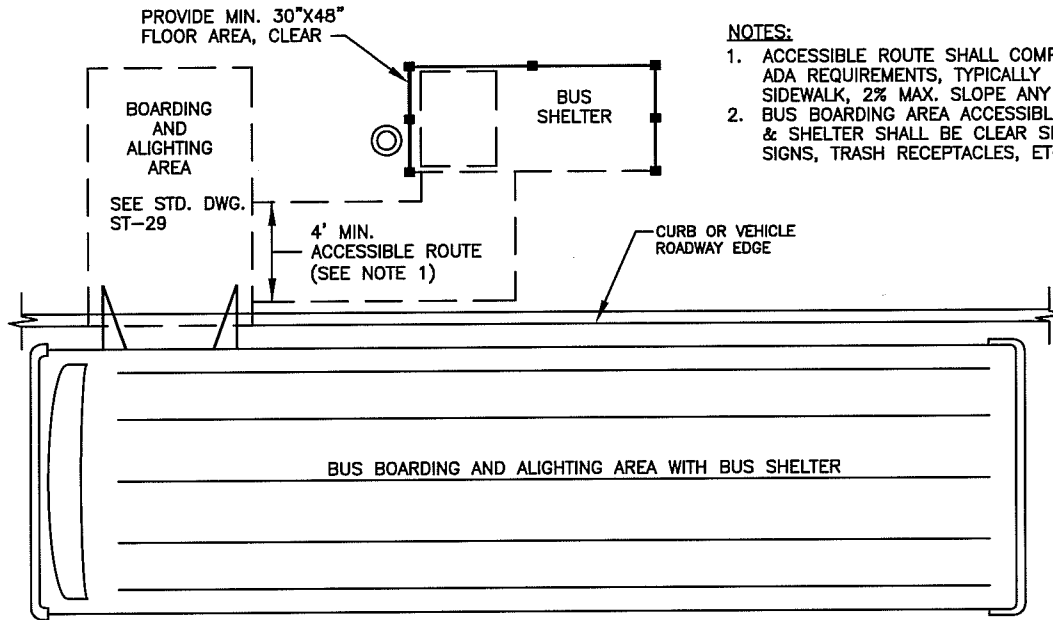
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DATE:

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SHEET 1 OF 1

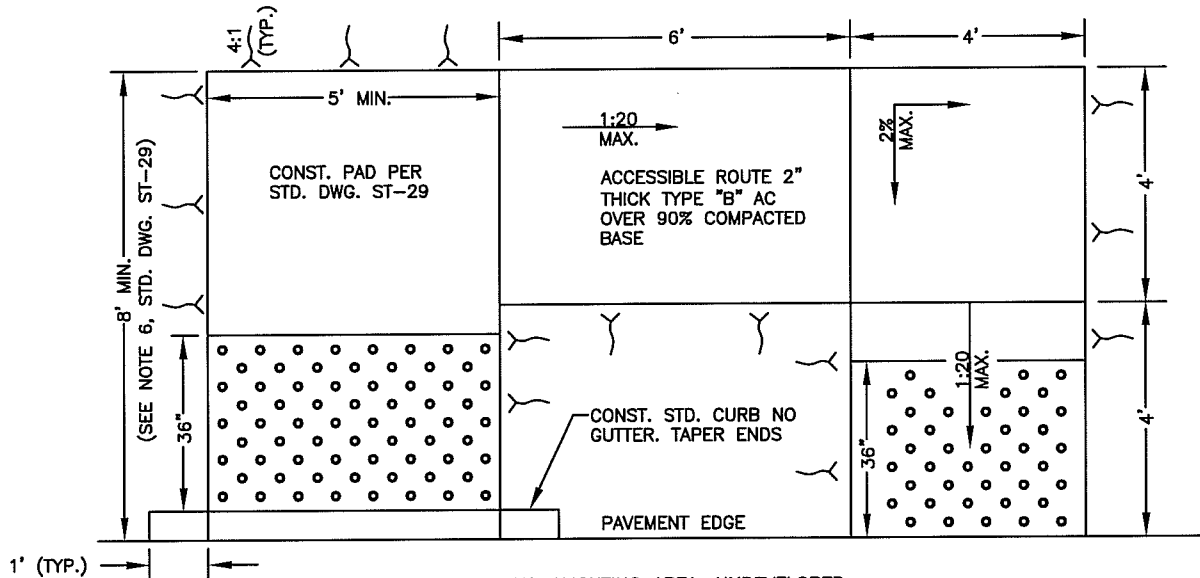


**NOTES:**

1. ACCESSIBLE ROUTE SHALL COMPLY WITH ADA REQUIREMENTS, TYPICALLY CONCRETE SIDEWALK, 2% MAX. SLOPE ANY DIRECTION.
2. BUS BOARDING AREA ACCESSIBLE ROUTE, & SHELTER SHALL BE CLEAR SPACE. NO SIGNS, TRASH RECEPTACLES, ETC.

**NOTES:**

- A. THIS TYPE OF BUS STOP SHALL ONLY BE USED IN UNDEVELOPED AREAS IN SPECIFIC RESPONSE TO INDIVIDUAL SPECIAL NEED.



BUS BOARDING AND ALIGHTING AREA, UNDEVELOPED LOCATIONS, NO CURB, GUTTER, SIDEWALKS (SEE NOTE A)



**CITY OF CLOVIS**

DWG NO.

**ST-29A**

**BUS STOP**

REF.

N.A.

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APPROVALS

SCALE: NTS

CITY ENGINEER

DATE:

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7/29/11

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01-14-11

BGJ

BGJ

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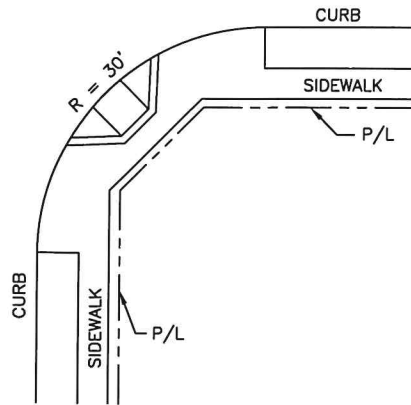
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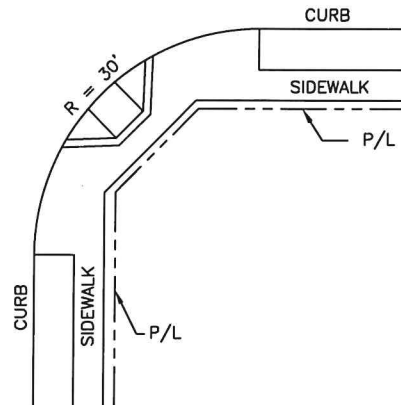
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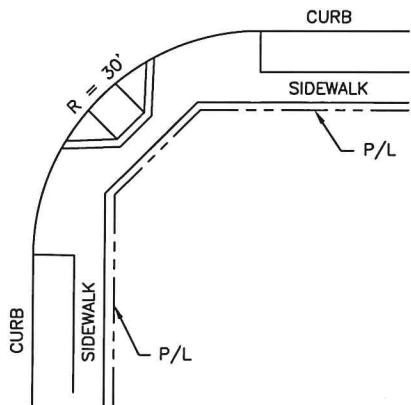
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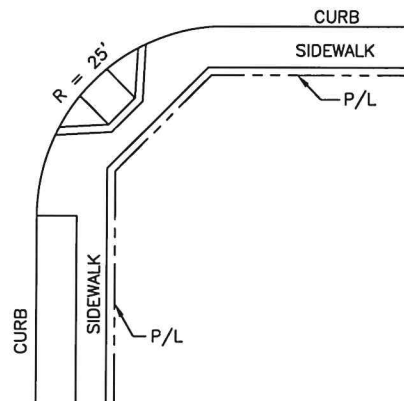
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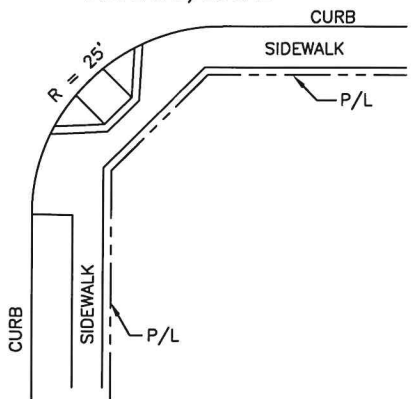
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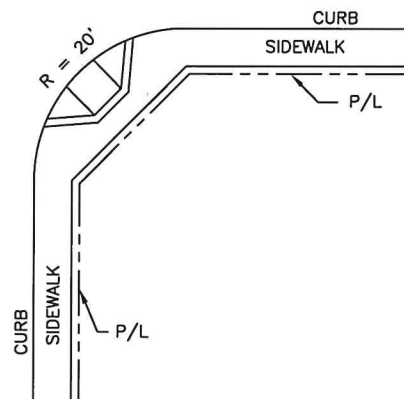
COLLECTOR/LOCAL



ARTERIAL/LOCAL



LOCAL/LOCAL



## NOTES:

1. RADII MAY VARY TO MEET LOCAL GEOMETRIC CONDITIONS.
2. CURB RAMPS ARE SCHEMATIC ONLY. EACH CORNER SHALL BE DESIGNED TO MEET ADA STANDARDS. SEE ST-11 FOR CURB RAMPS.



## CITY OF CLOVIS

DWG NO.

ST-31

## STANDARD CORNER RADII CONFIGURATION

N.A.

APPROVED BY:

CITY ENGINEER

DATE: 4/5/21

NO.

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APPROVALS

SCALE: NTS

06-18-09

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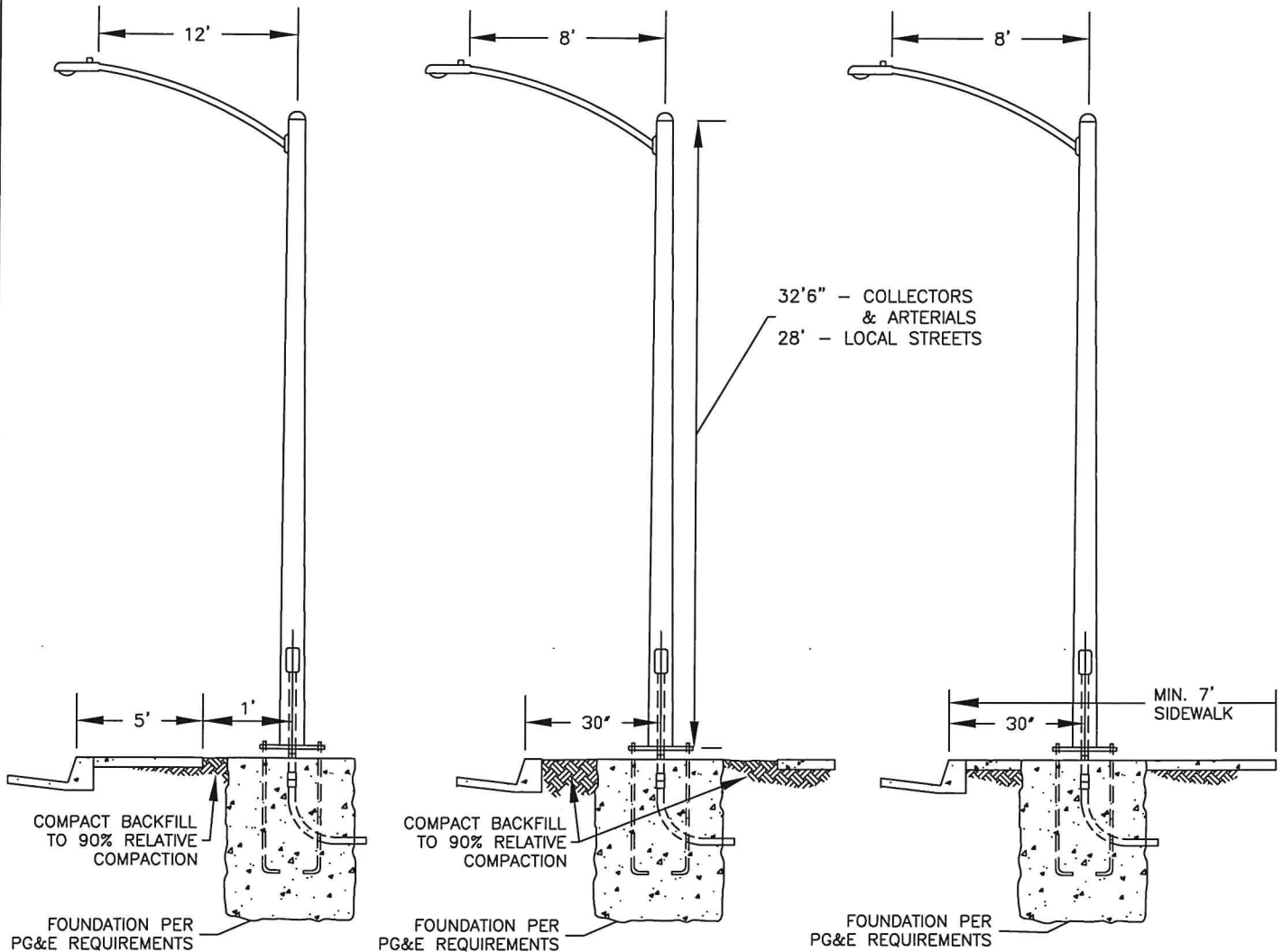
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SHEET 1 OF 1





**NOTE:**

INSTALLER TO VERIFY CURRENT PG&E STANDARDS.



# CITY OF CLOVIS

## STREET LIGHT LOCATIONS

DWG NO.

**ST-33**

REF.

N.A.

APPROVED BY:

*[Signature]*

CITY ENGINEER

DATE: 4/5/21

NO.

REVISED

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SHEET 1 OF 1

